

# JOERGER RANCH SPECIFIC PLAN

## TRUCKEE, CALIFORNIA



ADOPTED 3-24-15  
TOWN COUNCIL RESOLUTION No. 2015-09  
AMENDED 6-8-21

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# Chapter 1

## Introduction



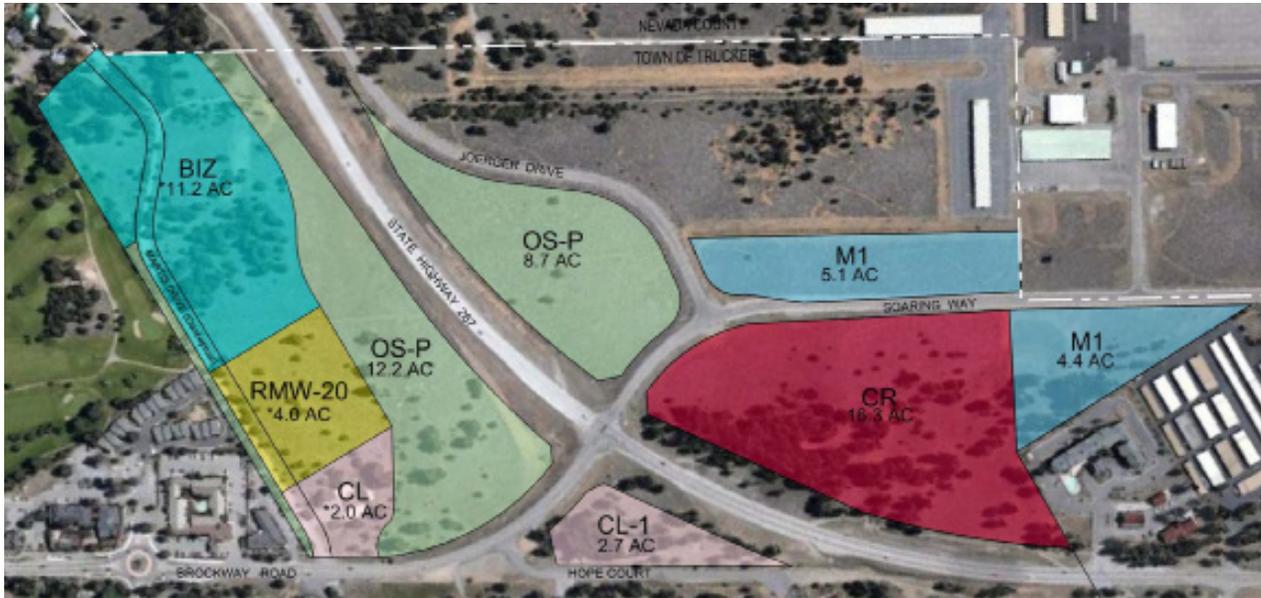


FIGURE 1-1: JOERGER RANCH ZONING MAP

\* ACREAGES EXCLUDE PROPOSED 60' RIGHT-OF-WAY FOR MARTIS DRIVE. LOCATION OF FUTURE MARTIS DRIVE IS SHOWN CONCEPTUALLY. ACTUAL LOCATION WILL BE DETERMINED DURING PROJECT-SPECIFIC SITE DESIGN FOR AREA WEST OF HWY 267.

## 1 | Introduction

The Joerger Ranch Specific Plan Area has a long history of development planning and buildout conceptualization. As part of the 2025 General Plan creation, guidance on future planning efforts was included in the Land Use Element for a range of commercial, industrial and residential land uses. Over the past ten years there have been numerous zoning and land use plans proposed for Planned Community-3 including New Urbanist and village concepts. Ultimately these concepts were abandoned in pursuit of a complementary but non-competitive zoning and land use plan that supports Truckee's historic core. An economic analysis prepared by Bay Area Economics in 2010 provided additional guidance on business types that would help diversify Truckee's economy without creating a new Downtown or passé strip shopping center. As part of the 2012 Draft Joerger Ranch Specific Plan and Draft Environmental Impact Report review, additional community input was provided. The comments received further shaped the specific plan into this

version which was adopted by the Town Council on \_\_\_\_\_.

Key concepts covered in the Joerger Ranch Specific Plan include economic diversification, regional business clustering and open space protection. Buildout of the plan area is envisioned to bridge the gap between the small specialty restaurant and retail uses Downtown—Truckee's heart and lifeblood—and the large big-box-anchored supercenter that would provide new sources of property tax and sales tax but fundamentally conflicts with Truckee's small mountain town character.

## 2 | Plan Area

The Joerger Ranch Specific Plan area is situated along one of only a few major travel corridors providing access to Downtown Truckee and North Lake Tahoe. The Truckee Tahoe Airport is located directly east of the site and Interstate 80 is located approximately 1.5 miles to the north. This location provides a

unique opportunity to foster high quality economic development that is both local- and regional-serving. The property size of 66.7 acres can accommodate uses that do not physically fit elsewhere in Truckee and would be best served in a regional location. The plan area also creates opportunity to relocate uses from Downtown including the Truckee River Corridor that are no longer compatible. Although airport operations and noise constraints from both the airport and the existing roadways limit the possibility of accommodating extensive residential uses in the plan area, there is opportunity for workforce and inclusionary housing in support of envisioned non-residential land uses. The Specific Plan Area is highly visible from State Route 267 and includes large sweeping views of the surrounding Sierra Nevada Mountains providing the opportunity to create a southern gateway entrance to historic Downtown Truckee.

### 3 | Purpose

This Specific Plan establishes zoning, design standards and guidelines for buildout of the plan area. The purpose of the Joerger Ranch Specific Plan is to provide the opportunity to strengthen and diversify Truckee's economy through fostering high quality development that can fulfill key General Plan economic development guiding principles including:

- Building upon the Town's existing assets to diversify and strengthen the local economy in ways that are appropriate and responsive to Truckee's context and natural environment.
- Developing high-wage jobs in Truckee that enable the local workforce to both live and work in the Town.
- Focusing business attraction, retention, and expansion efforts on key economic sectors that have the greatest likelihood of success in

Truckee.

- Maintaining and enhancing community quality of life as a key competitive advantage.
- Promoting and enhancing the Town's role as a year-round tourist destination (Economic Development Element Guiding Principle - 2025 General Plan).

### 4 | Authority

The Truckee General Plan designates the plan area as Planned Community 3 (PC-3) which is similarly designated as Planned Community (PC) on the Truckee Zoning Map. The purpose of the PC zone and General Plan land use designation is to require the preparation of a Specific Plan. The Specific Plan, called the Joerger Ranch Specific Plan after the Joerger family (original land owner) includes zoning, allowed land uses, development standards and guidelines for buildout of the plan area. This Specific Plan was prepared under the authority of the Truckee Development Code (Zoning Ordinance) with careful consideration to implement the goals and polices of existing Town planning documents.

The Joerger Ranch Specific Plan was prepared under the 2025 General Plan, the September 13, 2013 Development Code (Truckee Zoning Ordinance) and the August 6, 2010 Bay Area Economics Report. This Specific Plan serves as the zoning and land use authority for all development within the Joerger Ranch Specific Plan Area. The plan also serves as a tool to guide development in a manner that is consistent with Town planning and policy documents including the General Plan, the Trails and Bikeways Master Plan, and the River Revitalization Strategy. Any land use, subdivision or development approved in compliance with this Specific Plan is considered consistent with all other adopted plans including the General Plan.

## 5 | Specific Plan Overview

This Specific Plan is both a regulatory document and a tool to guide development of the Joerger Ranch Specific Plan Area in a manner that is consistent with Town planning and policy documents including the General Plan and Trails and Bikeways Master Plan. The vision chapter (Chapter 1) summarizes the general scale and character of anticipated development within the Specific Plan Area. Specific Plan standards and guidelines are provided throughout the plan to help regulate and guide the physical form of new development as well as desired land uses. Development Standards are mandatory and provide the minimum threshold for development. Design Guidelines are intended as a guide to assist property owners and project designers in understanding the Town's goals for attaining high quality development. Design Guidelines are preferred/recommend whereas Development Standards are mandatory and non-discretionary.

Development within the Joerger Ranch Specific Plan shall comply with the Town of Truckee Conditions of Approval adopted with Town Council Resolution No. 2015-09 (Specific Plan) and 2015-11 (Phase 1 Tentative Map). The Conditions of Approval for the Phase I Parcel Map are included in Appendix B and Mitigation Measures set forth in the certified Environmental Impact Report are included in Appendix C.

Truckee is a special community that prides itself on being a small, historic mountain Town and all development within the Specific Plan shall reflect this character. This Specific Plan provides areas of greater land use flexibility in comparison to the adopted Development Code, but also restricts some land uses to ensure implementation of the overall vision set forth in Chapter 2. The Specific Plan is organized into the following chapters:

**Chapter 1—Introduction:** Discusses the overall Specific Plan purpose; identifies the Specific Plan Area and explains the organization of the Specific Plan.

**Chapter 2—Vision:** Sets forth the vision of the Specific Plan, and lists key goals and policies to implement the vision.

**Chapter 3—Community and Regional Context:** Provides a description of the local and regional setting—both as it physically exists today and as it is planned for the future.

**Chapter 4—Land Use Standards and Guidelines:** Provides development standards and guidelines for development within the Specific Plan Area; includes the Zoning Map and Economic Cluster Overlay Map.

**Chapter 5—Transportation:** Describes the transportation and circulation plan and including vehicle, pedestrian, bicycle and transit systems.

**Chapter 6—Public Services and Infrastructure:** Describes existing infrastructure and the needed infrastructure improvements to serve development within the Specific Plan Area.

**Chapter 7—Implementation and Phasing:** Provides implementation measures and discusses project phasing within the Specific Plan Area.

**Chapter 8—Glossary**

**Appendix A - 2025 General Plan Consistency Matrix**

**Appendix B - Town of Truckee Conditions of Approval; Town Council Resolution No. 2015-09 and 2015-11 for the Joerger Ranch Specific Plan and Phase I Parcel Map**

• **Appendix C — Final Mitigation Monitoring and Reporting Program**

**Appendix D — Zoning Map**

**Appendix E — Phase I Parcel Map**

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# Chapter 2

## Vision & Goals



## 1 | Vision

Implementation of the Joerger Ranch Specific Plan will strengthen Truckee's small historic mountain town character by providing new commercial/retail, office, industrial and workforce housing opportunities at a key regional location. To implement this vision, this Specific Plan builds on the "economic clustering" of related businesses. The term economic clusters is defined in this plan as groupings of related businesses, suppliers and associated land uses within a set area. Each economic cluster is intended to draw businesses with similar and/or compatible land uses, operations, customers, etc. with the idea being that vision and innovation are often developed when there is open communication within an industry sector. Businesses within an economic cluster would support but not compete with Downtown and will create new sales tax revenue where dollars are currently being lost to other areas including Reno, Nevada. Buildings will relate to each other and have compatible design aesthetics, but the Joerger Ranch Specific Plan Area is not intended to draw business from Downtown or mimic the historic, pedestrian-focused core that makes Downtown unique.

The three economic cluster building blocks from which the Specific Plan is based include:

### *The Lifestyle Economic Cluster.*

This cluster encompasses the land located along Brockway Road and includes "Lifestyle Businesses," or businesses that benefit from association with the Town's image as a small historic mountain town. Businesses such as outdoor gear, businesses focused on health/fitness/wellness, businesses or organizations with an emphasis on conservation/sustainability or community are targeted for this area. The existing

Truckee River Winery is consistent with this cluster as a provider of recreational bocce courts and community event space. Implementation of this economic cluster would achieve economic goals of keeping Truckee competitive with adjacent ski resorts and attracting more local- and community-serving uses.

This Specific Plan includes two zoning districts to fulfill the Lifestyle Economic Cluster vision:

- **Lifestyle Commercial (CL) zone**
- **Lifestyle Commercial (CL-1) zone**

### *The Business Innovation Economic Cluster.*

This cluster is located at the northwestern portion of the plan area and is situated between the Lifestyle Economic Cluster and Highway 267. This cluster is intended to create a concentration of high tech, Research and Development, culinary, green technology and similar industries. Implementation of this economic cluster would draw year-round, full-time, high-quality jobs with living wages which will further diversify Truckee's economy.

This Specific Plan includes two zoning districts to fulfill the Business Innovation Economic Cluster vision:

- **Business Innovation Zone (BIZ)**—for the purpose of creating a campus setting and opportunities for shared resources
- **Workforce Multi-Family Residential, 20 dwelling units per acre (RMW-20)**—to provide workforce housing

### *Regional Economic Cluster.*

This cluster encompasses the land located along Soaring Way and creates land use opportunities for businesses with region-serving products

and services as well as relocated businesses from the Truckee River Corridor and other preferred in-fill sites Downtown. Implementation of this economic cluster would capture some of the retail leakage and sales tax dollars that currently go to neighboring jurisdictions including Reno, Nevada and would provide larger floorplate businesses that do not physically fit elsewhere in Truckee.

This Specific Plan includes the **Regional Commercial (CR)** zoning district to fulfill the Regional Economic Cluster Vision.

The Specific Plan allows development of the plan area to occur over time and in no specific order, as well as utilize existing services to provide flexibility to meet changing market and community demands (see Chapter 7 - Implementation & Phasing).

## 2 | Supporting Goals & Policies

The following planning goals & policies have been

established to guide implementation of the Joerger Ranch Specific Plan Vision:

### Joerger Ranch (JR) Goal 1

**Develop the Joerger Ranch Specific Plan area with land uses that promote high-quality economic development.**

- **Policy P1.1**  
Strengthen Truckee’s economic base through encouraging green industry, lifestyle businesses, and other commercial, office, manufacturing, and cultural uses that do not fit Downtown.
- **Policy P1.2**  
Projects with a minimum of 75% LEED™ for New Construction Silver buildings or better shall be refunded Planning Division land use processing fees upon proof of certification.

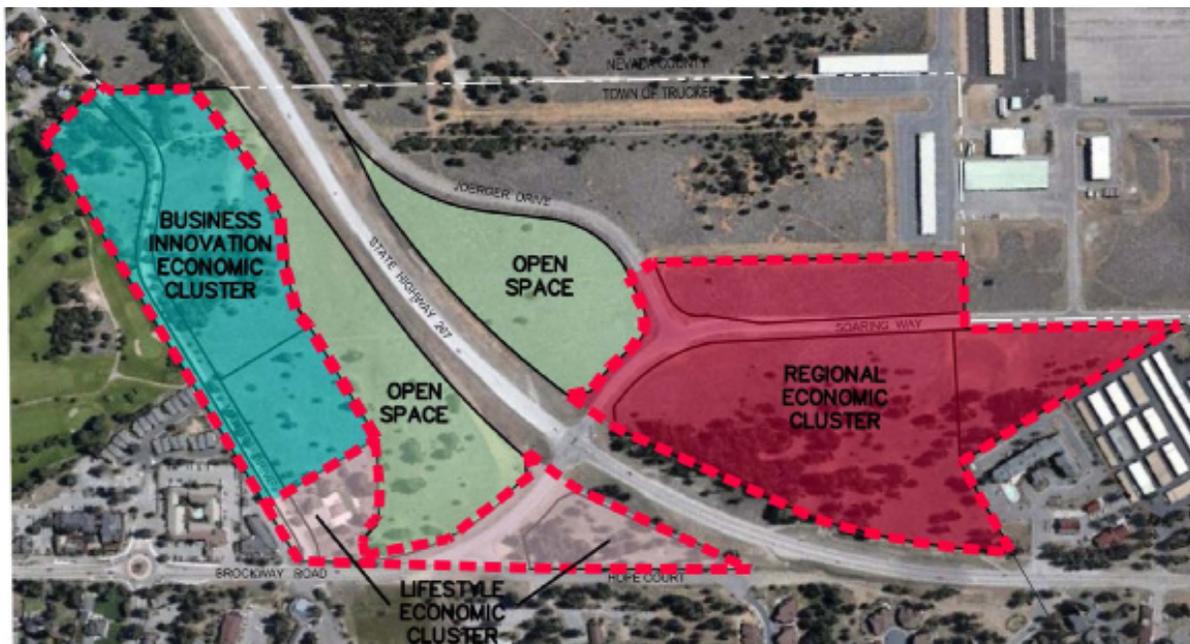


Figure 2-1: Economic Development Clusters

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## JR Goal 2

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**Encourage and incentivize relocation of manufacturing, industrial, processing and repair/maintenance uses from the Truckee River Corridor and Historic Preservation (-HP) overlay in-fill sites to the MI zone in the Specific Plan Area.**

- **Policy P2.1**  
Provide workforce housing in the Specific Plan Area through the creation of a RMW-20 (Multi-Family, 20 dwelling units per acre) workforce housing zone.

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## JR Goal 3

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**Extend development along the Brockway Road Corridor in keeping with the existing unique and eclectic character.**

- **Policy P3.1**  
Support the continued operation and expansion of the Truckee River Winery to enhance the outdoor living experience along the Brockway Road Corridor.

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## JR Goal 4

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**Provide a coordinated pedestrian and bicycle network to encourage non-motorized travel from surrounding neighborhoods and within the Joerger Ranch Plan Area.**

- **Policy P4.1**  
Streets within the Joerger Ranch Plan Area shall be constructed per the street sections which are shown within this Specific Plan.
- **Policy P4.2**  
Support pedestrian and bicycle linkages to

the Truckee River Legacy Trail, Riverview Sports Park and Regional Park.

- **Policy P4.3**  
Encourage connectivity between projects where feasible to further other goals, policies, and guidelines.
- **Policy P4.4**  
Two-stage left turn lanes shall not be used to mitigate a Level of Service deficiency, although they may be acceptable to construct in some circumstances to provide a consistent and compatible cross section along a corridor with low turning-movement volumes.
- **Policy P4.5**  
A roundabout on Brockway Road will be required to provide access to Parcel 4 if the side street LOS left-turn movements do not meet LOS standards as a two-way stop controlled intersection without a two-way left turn.

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## JR Goal 5

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**Balance open space preservation with economic diversification.**

- **Policy P5.1**  
Support open space preservation to the extent that key General Plan economic development goals for diversification, high wage job creation and business success can be achieved.
- **Policy P5.2**  
If Martis Road is relocated within the Parcel 4 open space, an equivalent amount of open space shall be replaced as an additional buffer along the northern and/or western boundary of Parcel 4, north of Brockway Road.

- **Policy P5.3**

All Open Space Protected (OS-P) zoned parcels shall be permanently preserved as open space in perpetuity.

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## JR Goal 6

**Foster economic diversification beyond tourism by encouraging high-tech businesses to locate in the Business Innovation Zone.**

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## JR Goal 7

**Encourage the development of new regional-serving uses that reflect Truckee's unique small historic mountain town character.**

- **Policy P7.1**

Encourage locally-owned and operated businesses.

- **Policy 7.2**

Target businesses that benefit from association with the Town's image as a small historic mountain town—such as the outdoor gear industry, health, fitness, and wellness, and fast casual dining restaurants catering to travelers and motorists in the area.

- **Policy P7.3**

Development within the CR zone is intended to supply retail service needs of the local population and recreational tourist population that frequents the area. Grocery market and/or specialty food markets along with complimentary ancillary uses are the intended goals. Unique building design and site orientation shall be required so as to avoid typical big box retail found in most urban areas.

In order capture the retail leakage, avoid

competition with Downtown and discourage big box chain retail providers, an economic impact analysis shall be conducted for any single retail use with a Gross Floor Area of 15,000 S.F. or larger (excluding grocery markets and specialty food markets). At a minimum, the economic impact analysis shall:

1. Define the market area.
2. Identify retail leakage that could be captured while still maintaining the desired intent of the CR zone.
3. Identify potential economic impacts on Downtown and the Town of Truckee as a whole.
4. Identify potential uses that would benefit the local economy while still complying with the use and design intent listed above.

- **Policy P7.4**

In order to avoid vacant building and degradation of existing retail centers within the Town, a proposal for the reuse of any retail sites with more than 15,000 sq. ft. of Gross Floor Area shall be submitted with the land use application. The proposal shall include design features that demonstrate availability of flexible features such as partitions, multiple entryways, etc. to facilitate reuse by multiple tenants if the building is abandoned. The plan for reuse shall be considered by the Planning Commission concurrent with the Economic Impact Analysis required by Policy 7.3 above.

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## JR Goal 8

**Ensure the construction of workforce housing consistent with the intent and purpose of Development Code Chapter 18.216 (Workforce Housing).**

- **Policy P8.1** - Development within the Specific Plan Area with the exception of the construction of workforce housing units on the RMW-20 parcel shall comply with the requirements of Development Code Chapter 18.214 (Inclusionary Housing).
- **Policy P8.2**  
The number of required workforce housing units for the Specific Plan Area are calculated in Table 7.4A based on maximum development potential of 0.25 Floor Area Ratio (FAR). The RMW-20 zone combined with payment of in-lieu fees listed in Table 7.4B, fulfills the workforce housing obligations for all future development within the Specific Plan area.
- **Policy P8.3**  
A minimum of 18 dwelling units per acre (72 units) and a maximum of 20 dwelling units per acre (80 units) shall be constructed in the RMW-20 zone.
- **Policy P8.4**  
A maximum of 20% of the total workforce housing units may be 1,000 sq. ft. with no limitations on the number of bedrooms. The remaining units shall be a mix of studio, one-, and two-bedroom units.
- **Policy P8.5**  
Housing in-lieu fees, as identified in Table 7.4B, shall be paid prior to issuance of temporary occupancy for the first building within each zoning district for projects approved prior to adoption of the Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments. Future projects shall comply with the requirements of Development Code Chapter 18.216 (Workforce

Housing).

- **Policy P8.6**

Foundations shall be installed for a minimum 72 unit workforce housing project within the RMW-20 zone prior to buildout of 40% of the Specific Plan (139,828 sq. ft. of residential and non-residential floor area), and adequate performance guarantees shall be provided to ensure completion of the workforce housing construction in compliance with Development Code Chapter 18.84 (Performance Guarantees).

- **Policy P8.7**

Prohibit the issuance of building permits once a total of 139,828 sq. ft. of residential and non-residential floor area has been constructed if foundations have not been installed and adequate performance guarantees provided for the workforce housing project.

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## JR Goal 9

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**Identify and meet the transit needs within the Joerger Ranch Specific Plan Area and contribute toward the long term success of transit operations.**

- **Policy P9.1**

Prior to future subdivision or development, the developer shall become members of the Truckee North Tahoe Transit Management Association. This membership shall remain in effect as long as transit services are provided within the Joerger Ranch Plan area. If creation of a future homeowners association or commercial property owners association occurs during development of any residential or commercial phase of the project, this membership may be assumed by the owners association's request of Truckee North Tahoe Transit Management Association.

- **Policy P9.2**

Prior to future subdivision or development (within the CR or BIZ zones), a transit plan shall be prepared to identify transit needs and recommended service for the project. The plan shall also identify the project's fair share for expanded transit service and a method for financing the expanded service.

- **Policy P9.3**

All development projects within Joerger Ranch Specific Plan shall provide on-going funding for transit operations. The amount and mechanism shall be approved by the Town Engineer.

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# Chapter 3

## Community & Regional Context



## 1 | Introduction

This chapter frames the Joerger Ranch Plan Area within the context of the neighboring local and regional context — both as it physically exists today and as it is planned for the future. This chapter also describes the various jurisdictions, agencies and special districts having regulatory authority over the Plan Area.

An economic evaluation prepared by Bay Area Economics (BAE) was used to define the types of commercial retail/business park, light industrial and manufacturing land uses that could be accommodated in the PC-3 Specific Plan. The intent is to a) not compete with Downtown Truckee commercial retail, b) provide land area with zoning that can accommodate possible relocation of certain types of existing commercial and industrial businesses located along the Truckee River corridor, and c) capture certain types of commercial / retail, business park, light industrial and manufacturing land uses that local residents currently travel to Reno to purchase. The report concluded the following:

- Retail space for PC-3 should be restricted to serving the large users that would not physically fit into downtown Truckee's historic building spaces. This would include targeting businesses that need to move large products, like furniture and appliances, or other uses that may have trouble transporting goods in and out of downtown Truckee. In addition, PC-3 could provide sufficient land area to allow businesses currently located on the Truckee River the opportunity to relocate to more a compatible use area. This will allow the Town to implement the vision of the Truckee River Revitalization Plan. The report also suggested that while downtown Truckee is a local regional dining destination, PC-3 may represent a suitable

location for certain restaurant types that would not be appropriate in the downtown, such as fast food or “chain” casual dining restaurants.

- There is a demand for nearly 270,000 S.F. of additional retail space that Truckee could be absorbed through 2023. That demand will be in the areas of apparel, general merchandise, food stores, eating and drinking places, home furnishings and appliances, and building material categories. During this timeframe, the Town is projected to be able to absorb the equivalent of one more full-size supermarket or a couple of smaller format food stores, up to two service stations and possibly a small recreational vehicle dealership selling motorcycles, snowmobiles, ATVs boats and/or personal watercraft.
- The analysis found that smaller scale developments, such as medical and professional offices and most retail uses, are more suitable and could be more easily absorbed within the other four planned development areas (PC-1, PC-2, Railyard & Hilltop Master Plan), as well as in existing commercial spaces in Truckee. Uses requiring larger spaces, such as large floor plate office users, fitness and health clubs, light manufacturing, green construction and retrofit businesses, alternative and renewable energy generation, R&D facilities and small recreational vehicle dealerships, would most likely encounter greater regulatory restrictions in the other four planned development areas and existing commercial spaces in Truckee, and may be more suitable for the PC-3 property.

- PC-3 presents a unique opportunity to house “green sector uses” within Truckee without competing with the existing and other planned commercial developments. Green sector uses could include facilities for businesses involved in energy conservation and alternative energy as well as outdoor environmental sustainability culture (whether in research and development or manufacturing).
- PC-3 may also be an opportunity for vehicle service stations, larger format retail, chain restaurants not appropriate for the downtown, and grocery markets.

Given these factors, the zoning and targeted uses proposed in the Joerger Ranch PC-3 Specific Plan are intended to promote economic activity and create local jobs while not competing with the economic base of Downtown Truckee. They also can promote redevelopment along the Truckee River.

The BAE report concluded that PC-3 could be developed in such a way as to lessen the potential competitive impacts on downtown, diversify the local economy and promote economic development by targeting the following:

- Retail space that emphasizes buildings larger than 5,000 S.F. This would allow for expanded economic opportunities and limit competition with the Downtown area, since buildings Downtown tend to be smaller than 5,000 S.F.
- Commercial uses that need larger building area, such as indoor recreation and training facilities, health and fitness clubs, base facilities for environmental tourism (i.e. biking, rafting, boating, camping expedition equipment, storage, etc.) culinary center

and grocery markets.

- Businesses like furniture or appliance dealers who require large areas for showrooms or storage along with areas for truck loading and unloading and delivery of goods.
- Fast food and/or chain casual dining restaurants that tend to locate in conventional shopping center settings near highways.
- Large floor plate users that cannot be accommodated within the downtown Truckee area due to the existing building limitations and inadequate available square footage.
- Recreational vehicle dealership, repair and maintenance facilities, boat, motorcycle, ATV, snow mobile sales and auto sales.
- Warehousing that can support custom furniture or fabrication operations, greenhouse and specialty food production operations, light manufacturing and Research and Development.

## 2 | Truckee & North Tahoe Region

As a gateway to the north shore of Lake Tahoe (via both State Routes 267 and 89), Truckee has an opportunity to serve a large population of tourists and travellers passing through to the North Tahoe region and surrounding areas. State Route 267 crosses through the middle of the Plan Area and provides the most direct route from Interstate 80 (I-80) to the north shore communities of Incline Village, Tahoe Vista, Kings Beach and Carnelian Bay. This results in the Joerger Ranch Plan Area having a regional market significance.

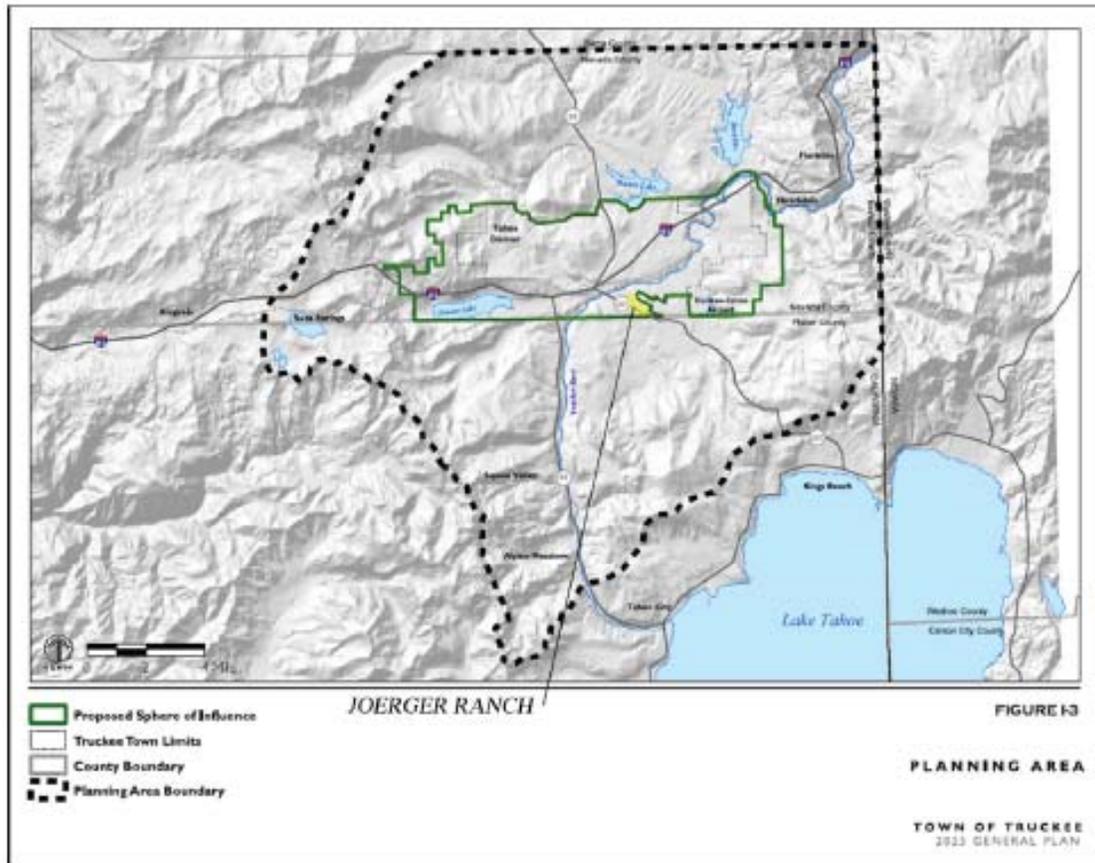


FIGURE 3-1: TOWN OF TRUCKEE PLANNING AREA

The Martis Valley Community Plan (MVCP, Placer County, adopted in 2003) considers the Town of Truckee the urban core of the Martis Valley, an area of land approximately seventy square miles adjacent to the Town's southern jurisdictional boundary. Joerger Ranch, situated at the northern edge of the Martis Valley and southern edge of the Town of Truckee, is a primary location for providing services and goods to support the full-time, seasonal and tourist population.

Joerger Ranch provides an accessible and convenient location to obtain services and goods for a large area of the Truckee community, as well as the region south of Town, given its location in the southeast area of the Town and the existing highway and local roads crossing through the Plan Area. Joerger Ranch is easily accessed from nearby neighborhoods including Sierra Meadows and

Ponderosa Ranchos, as well as the more distant neighborhoods of Glenshire, Prosser Lakeview and Tahoe Donner (these three neighborhoods representing nearly 10,000 residential units). The same is true for the nearby Martis Valley (Placer County) neighborhoods of Lahontan, Schaffer's Mill, Martis Camp and Northstar. Joerger Ranch enables residents and visitors of these neighborhoods to conveniently access services and goods, resulting in reduction of vehicle miles traveled to alternative service commercial locations.

### 3 | Local Community

#### 3.1 Neighboring & Surrounding Land Uses

The Joerger Ranch Plan Area is directly adjacent to a wide variety of existing and planned land uses. The Truckee-Tahoe Airport, a general aviation facility,

Table 3-1 – Land Uses Adjacent to Plan Area

ASSESSORS PARCEL NUMBER	WEST	EAST	NORTH	SOUTH
19-620-01	Ponderosa Golf Course	APN 19-620-02	Open space, low density residential	Ponderosa Golf Course
19-620-02	Martis Drive, Ponderosa Golf Course	SR 267	Open space	Brockway Road
19-620-31	SR 267	Joerger Drive	Truckee-Tahoe Airport	Brockway Road
19-620-04	SR 267	Truckee Tahoe Airport	Brockway Road	SR 267

Source: Quad Knopf, Inc. – Table 3.1.1 of O&C Analysis

is the major existing land use north and east of the Plan Area. Areas north, west and south of the Plan Area are characterized by a mix of low and medium density residential, commercial and recreational uses. The Ponderosa Golf Course borders a portion of the Plan Area directly to the west. Table 3-1 lists the existing land uses that border each of the four parcels comprising the Joerger Ranch Plan Area. Figure 3-2 indicates the existing uses surrounding the Plan Area.

Following the variety of land uses directly adjacent to the Joerger Ranch Plan Area, the immediately

surrounding land uses (e.g.: those in close proximity, but not adjacent to, the Plan Area) and neighborhoods also represent a diverse, and distinctly different, set of land uses. The area west of the Plan Area is dominated by single and multiple family residential land uses on both sides of Brockway Road, known within the Town General Plan as the Brockway Road Corridor. This corridor is also characterized by open space and recreation lands as well as a variety of local-serving commercial uses fronting Brockway Road. The Truckee Tahoe Airport occupies a vast majority of the land area to the east of the Plan Area, with a range of office, commercial (e.g.: retail and service), industrial (e.g.: warehousing and storage) and public (including Truckee’s Town Hall) uses along the east-end of Soaring Way and Truckee Airport Road. A very similar land use pattern exists along Business Park Drive, a local connector road between Truckee Airport Road and Soaring Way.

Nearby the Plan Area, the Truckee-Tahoe Airport lands are the predominate lands



FIGURE 3-2: SURROUNDING USES

to the east. An established single family residential area surrounding the Ponderosa Golf Course lies to the northwest of the Plan Area. Interstate 80, the Truckee River and the Union Pacific railroad are located approximately one half mile north of the Plan Area, just beyond the Truckee-Tahoe Airport. To the south, the nearby area is characterized by residential and commercial uses on either side of SR 267 for approximately one-quarter mile. Further south, uses along SR 267 quickly transition to the open space of the Martis Valley beyond the Town of Truckee and Placer County boundary.

### 3.2 Relationship with Town Neighborhoods, Centers & Commercial Centers

The General Plan Community Character Element places particular emphasis on the importance of small town mountain character and town design. Town design is defined in the Community Character Element (GP Page 3-10) as follows:

*Town Design focuses on the visual and functional relationships of the places, buildings and structures that define the built environment. It considers the relationships between people and places, circulation and connections, and the physical form of the town. Understanding community character in Truckee comes, to some extent, through an understanding of the composite parts of Truckee’s built environment, the “building blocks” of centers, neighborhoods, districts, corridors and gateways that, in addition to the open spaces described previously, comprise the fabric of the town. To provide a framework for discussing issues of community character as it relates to the form of the built environment, this Element looks at Truckee in terms of a series of discrete places and community areas, categorized by type. These types include:*

- **Centers**, the commercial and mixed use

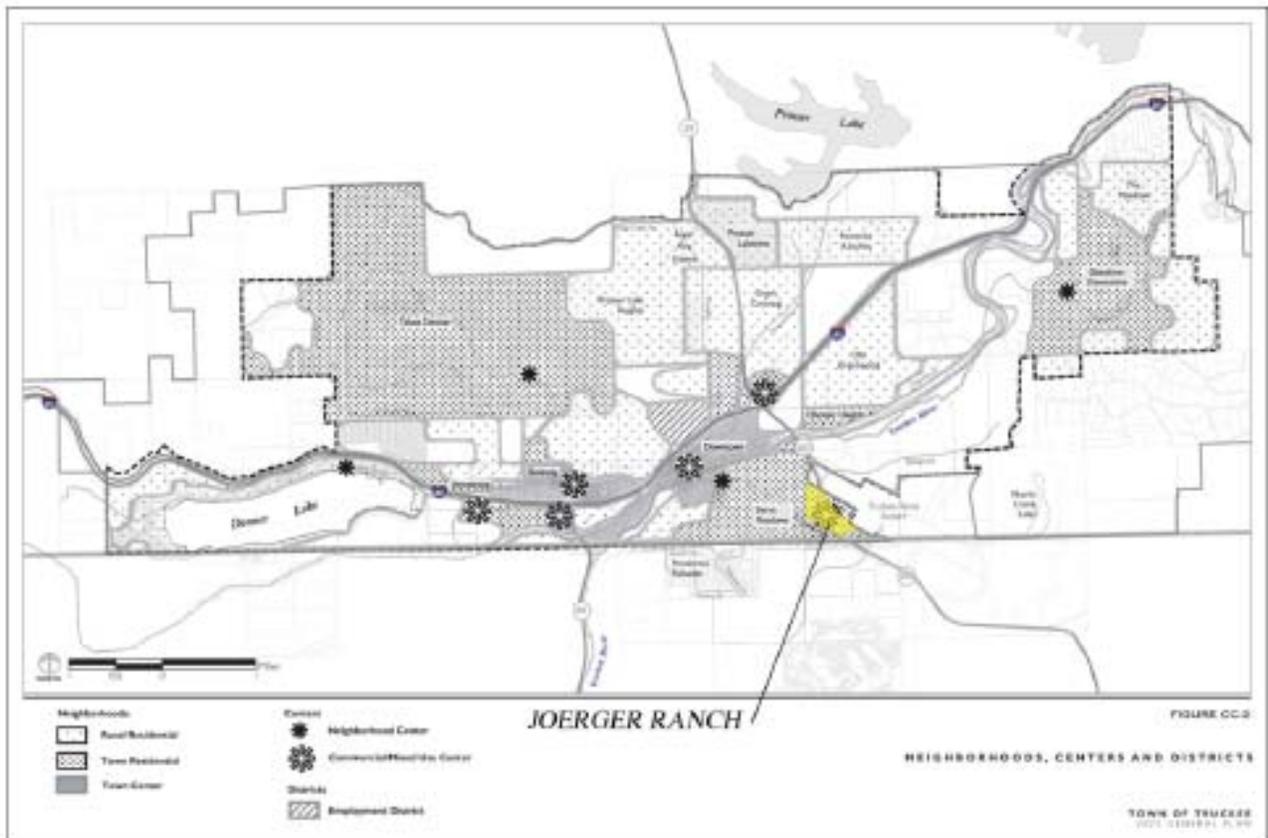


FIGURE 3-3: TOWN’S NEIGHBORHOODS, CENTERS AND DISTRICTS

areas of the Town that serve as the focus for community life and commercial activity.

- **Residential Neighborhoods**, which constitute the majority of Truckee’s developed areas.
- **Employment Districts**, which include the relatively few places in Truckee where job-generating uses dominate.
- **Corridors and Gateways**, the linear features of the town that provide both connectivity and identity to the community at its entry points.

Joerger Ranch embodies all four of these types of places. As a result, it plays a significant role in strengthening the Town’s fabric. The General Plan designates the portion of the Plan Area adjacent to Brockway Road as in the Brockway Road Neighborhood Area Overlay (see Figure 2-3). This designation is in recognition of the Brockway Road

corridor’s importance as a key gateway to Truckee from the south.

The General Plan’s Brockway Road Policy 1 (GP Page 2-43) seeks the preservation and improvement of the existing corridor through respecting its “open qualities”, recognizing its “gateway status” and supporting its use as a “bicycle and pedestrian route”. The General Plan designates the Town’s important gateways and corridors (see Figure 2-4) and identifies the Brockway Road Corridor and its intersection with SR 267 as important to the Town’s overall community character. The Town’s General Plan addresses the Brockway Road Neighborhood Area, including reference to PC-3, as follows (GP Pages 2-42 and 2-43):

*The Brockway Road Neighborhood Area is applied to the Brockway Road Corridor, in*

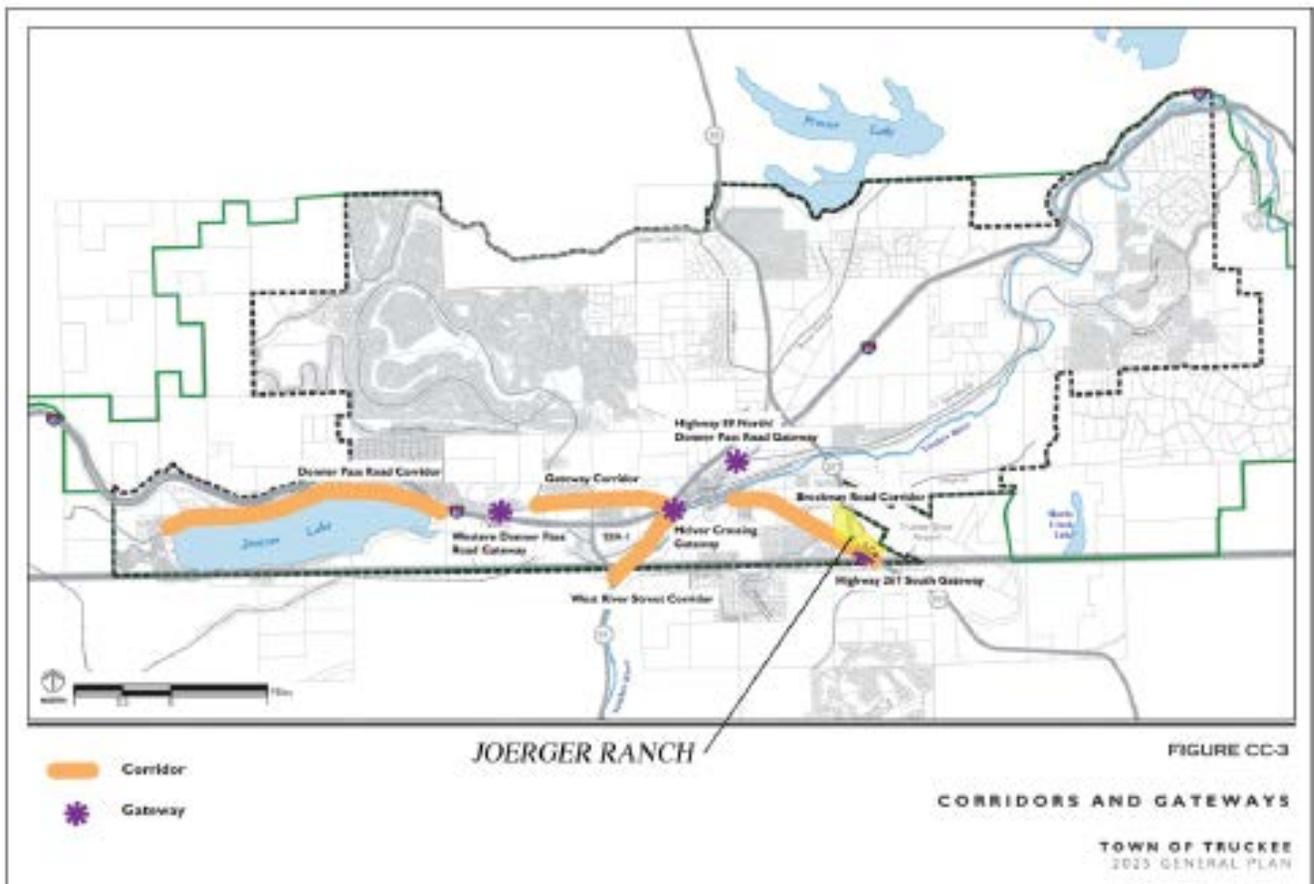


FIGURE 3-4: TOWN’S CORRIDORS & GATEWAYS

*recognition of this important thoroughfare as a key gateway to Truckee from the south. It also recognizes the potential for significant change in this area with conversion of the roadway from a State Highway to a local road, following the completion of the Highway 267 Bypass.*

*Land Uses Allowed: Land uses allowed in this designation are those uses permitted in the underlying land use designations. These include residential uses, predominately in the medium and high-density ranges; commercial and lodging uses; public facilities; open space for recreational uses and scenic and habitat value; and uses allowed under an approved specific plan for PC-3.*

*Density and Intensity: Residential designations, as shown in the Land Use Map, allow development at densities ranging from three to twelve units per acre. Commercial and industrial uses are allowed at an average FAR of 0.2.*

The General Plan identifies Town's neighborhoods, centers and districts, including the identification of the Town's two largest Town Centers: Downtown Truckee and the Gateway Area (see Figure 2-4). Although not specifically identified as an employment district, it is appropriate to consider Joerger Ranch within this classification, recognizing the considerable job-generation resulting from development of the Plan Area. Joerger Ranch is not considered to be a Town Center, rather one of six Commercial/Mixed Use Centers within the community. This is an important distinction, as the Commercial/Mixed Use Center classification of the General Plan guides the type of place for Joerger Ranch and its relationship with the larger community.

### 3.3 Relationship with Downtown Truckee

Downtown Truckee maintains an important distinction from other Town centers and commercial / mixed-use centers as the "historic and commercial heart of the community" (GP Community Character

Element, page 3-12). Although Joerger Ranch is located more than a mile east of Historic Downtown Truckee via Brockway Road, there is a reasonable physical relationship between these two important areas of Town, particularly with pedestrian and bicycle linkages. The Town's General Plan Land Use Policy 6.2 necessitates a complementary relationship between Downtown Truckee, as a Town Center, and Joerger Ranch, as a Commercial/Mixed-Use Center. That policy states:

*Maintain and enhance Downtown as the heart of Truckee and as the Town's premier tourist destination through the following methods, and through Action A6.2.*

Furthermore, the importance of maintaining the economic health of Downtown Truckee by identifying and understanding potential competition created by PC-3 and other "large developments with substantial commercial/industrial floor space" (GP Land Use Element, Action A6.1, page 2-58), is a high priority for the community. The Downtown area has its own distinctive character, ambiance and attraction. The commercial land use allowed by this Plan emphasizes community serving uses which are more appropriately located outside of Downtown Truckee, due to the type of uses and large floor area necessary to accommodate these uses. As required by GP Land Use Element Action A6.1, an economic analysis was prepared for the Specific Plan. The findings of the report identify appropriate uses.

## 4 | Plan Area Environmental Setting & Conditions

### 4.1 Land Use

The existing site is primarily undisturbed and undeveloped, with the exception of one commercial building and an adjacent abandoned well with a pump house structure. The small commercial building and



1 | VIEW NORTH FROM HWY 267

In general, the Plan Area is composed primarily of sagebrush scrub with scattered pine around the site and in clusters on the southern portion of the Plan Area on either side of SR 267.

pump house structure are located near the southwest corner of the project site and accessed from Brockway Road. There are significant roadway and drainage infrastructure improvements that bisect the property.

**4.2 Visual / Aesthetic**

The Plan Area is located in the Martis Valley, a large, level-to-rolling meadow at the confluence of the Truckee River and Martis Creek floodplains, east of Interstate 80 (I-80) and the Town of Truckee. The valley-bottom portions of the site are visually open, with views over large areas of open meadow interrupted by substantial stands of Ponderosa pine. Views from the valley to nearby peaks and ridges of the Sierra Nevada and Carson Ranges are visible in all directions. Visual quality of the Martis Valley, despite existing development, is high. Mountains and wooded slopes remain a predominate part of the landscape. The site has also been identified in the General Plan as the key gateway to the Town of Truckee from the south.

**4.3 Biological**

The natural vegetation within the Plan Area consists of patchy grasslands and meadows, sagebrush shrublands, and coniferous forests and woodlands.

A search of the California Natural Diversity Database (CNDDDB) identified eleven species within a five mile radius of the Plan Area. The Plan Area contains no natural communities considered sensitive or rare by the California Department of Fish and Game.

The site contains two drainages that were determined to be isolated waters (ephemeral and intermittent streams). These isolated waters are fed from the south by culverts passing under Brockway Road and State Route 267.

**4.4 Cultural**

Two cultural resources site were identified by Kautz Environmental Consultants, Inc. however, these two sites have no meaningful relationship to any of the historic themes identified for the project area: transportation, timber, ice or tourism. The sites have no known relationship to any historically significant person or event and are considered non-significant.

**4.5 Geological**

Slopes within the Plan Area are generally flat to gentle with existing grades ranging from approximately 5,850 feet above mean sea level (MSL) in the northwestern portion of the site to approximately 5,930 feet MSL in



2 | VIEW SOUTH FROM HWY 267



3 | VIEW EAST ALONG BROCKWAY ROAD

the southern portion of the site. The Plan Area is located along the western edge of the Truckee Basin in the Martis Valley. The Truckee Basin was filled with volcanic flows and sediments of the Lousetown Formation, known as the Martis Valley formation of Latham.

#### 4.6 Hydrologic

There are no FEMA-mapped 100-year flood plains within the Plan Area. Generally, surface runoff enters the southwest quadrant of the Plan Area through a culvert under Brockway Road and is absorbed onsite.

In the northwest quadrant of the Plan Area, it appears that significant surface runoff enters from the south, through a culvert under State Route 267 and continues off-site to the adjacent parcels to the north-northeast. A drainage ditch runs along a portion of the Truckee Tahoe Airport's southwest boundary east of Joerger Drive. Additional information regarding hydrology and storm drainage characteristics is provided in Chapter 5.



4 | VIEW WEST @ HWY 267 &amp; BROCKWAY ROAD

#### 4.7 Traffic & Circulation

##### INTERSTATE 80 (I-80)

I-80 provides interregional highway connections east to Reno, Nevada and beyond, and west to Sacramento, California and the San Francisco Bay Area and is a main transportation route between northern Nevada and the Bay Area. The Martis Valley area lies to the south of I-80, thirty-four miles west of Reno and ninety miles east of Sacramento. This section of I-80 is currently a four-lane divided highway with speed limits posted at sixty-five miles per hour.

The Joerger Ranch Plan Area is located along State Route 267, approximately 1.3 miles southeast of the I-80 / State Route 267 / State Route 89 interchange. I-80 is a significant transportation route to the Plan Area with travel times less than 2 minutes from the interstate.

##### STATE ROUTE 267

State Route 267 is a two-lane highway running in a general northwest-southeast alignment between the I-80/State Route 89 North/State Route 267 interchange in Truckee and State Route 28 in Kings Beach. From the I-80/State Route 89 North/State Route 267 signalized intersection, the roadway heads south across the State Route 267 Bypass (completed in November 2002).

State Route 267 is of local and regional significance, providing access to residential, industrial, commercial and recreational land uses. The Bypass provides a direct travel route for regional traffic between



5 | VIEW TO EAST FROM HWY 267 SOUTHBOUND

the Martis Valley and the Tahoe Basin, connecting Interstate 80 in Truckee to the Northstar at Tahoe ski resort and Kings Beach on Lake Tahoe.

State Route 267 also has signalized intersections at Brockway Road/Soaring Way and the Truckee Airport Road intersections.

**STATE ROUTE 89**

State Route 89 (SR 89) is one the three primary California routes that access Lake Tahoe (the other two are State Route 267 and U.S. Highway 50). SR 89 provides access between Donner Pass Road in Truckee and Tahoe City (the SR 89 south segment). Starting at the I-80/State Route 267 interchange on the east side of Truckee, SR 89 north services as a rural two-lane highway connecting Truckee and Sierraville, Quincy, Mount Lassen National Park and Mount Shasta to the north.

**BROCKWAY ROAD**

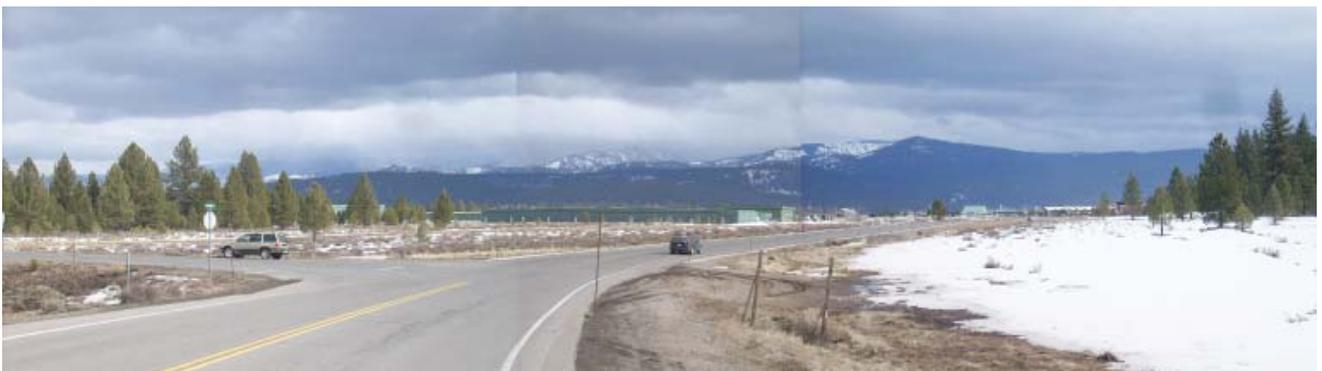
Brockway Road connects the Bridge Street/South River Street intersection in downtown Truckee

southeastward to State Route 267 at the Brockway Road/Soaring Way intersection located just north of the Placer County/Nevada County line. Brockway Road is a two lane arterial roadway with speed limits



6 | VIEW FROM INTERSECTION OF JOERGER DRIVE / SOARING WAY

varying from twenty-five mph to forty-five mph. Traffic signals are currently installed at the Palisades Drive intersection and the intersection with State Route 267, while the Martis Valley Road/Brockway Road intersection is controlled by a roundabout. All other intersections are controlled by stop signs on the side street approaches.



7 | VIEW TO EAST FROM SOARING WAY @ JOERGER DRIVE

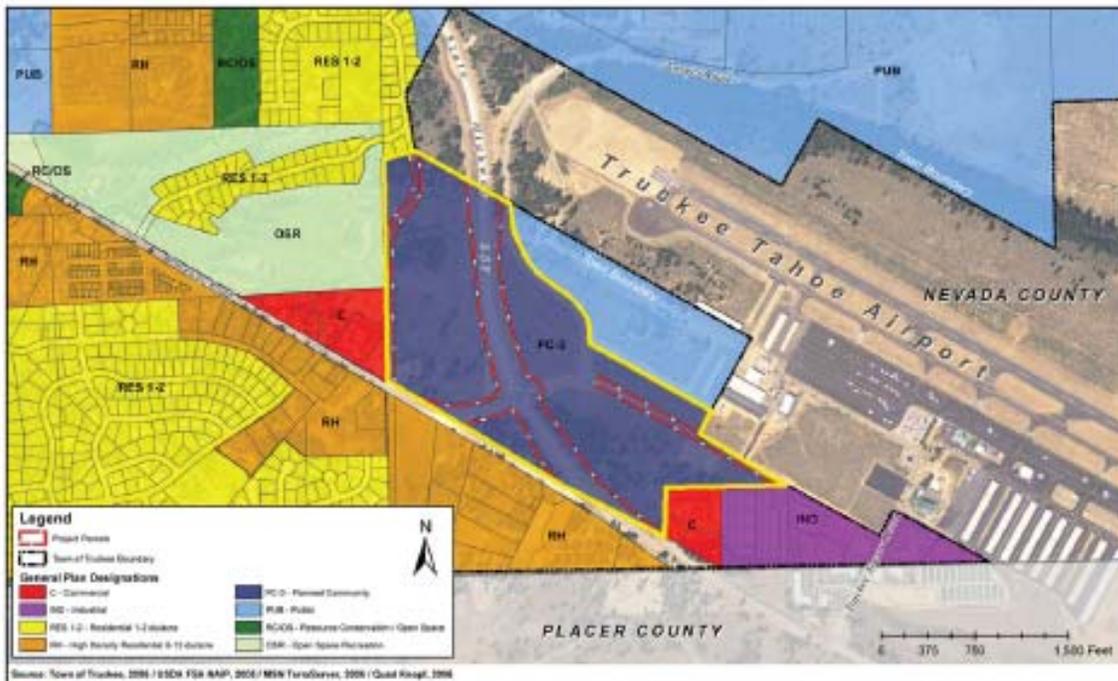
**DONNER PASS ROAD**

From its eastern terminus at State Route 89 north, Donner Pass Road travels westward to Donner Lake and Soda Springs. This roadway is a vital link for local circulation by providing access to historic Downtown Truckee, public and commercial uses in the Gateway areas such as the Tahoe Forest Hospital, Gateway Commercial Center and several school facilities, as well as Donner Lake State Park and the Donner Lake residential area. This roadway provides a single through lane in each direction, with a continuous center left-turn lane along the segment between Levone Avenue and Northwoods Boulevard.

The General Plan is the long term policy guide for the physical and economic growth and environmental protection of the Town of Truckee. The Land Use Element (GP Page 2-26) requires preparation of a Specific Plan for PC-3 and development to be consistent with this Specific Plan. This Chapter references many key General Plan policies and designations applicable to Joerger Ranch in describing its relationship with the larger community. Many additional General Plan policies are applicable to Joerger Ranch, each summarized in the General Plan Consistency Matrix (see Appendix A). As a General Plan implementation tool, the Specific Plan establishes an important link between the General Plan and development proposals for the Joerger Ranch Plan Area by allowing for further planning and analysis of large, important areas of Town that would not be provided at the General Plan level. Additional planning and analysis is necessary for the Planned Community sites because their proper

**5 | Relationship to Other Plans & Regulatory Documents**

**5.1 2025 General Plan**



**PC-3 / JOERGER RANCH**  
**CURRENT TOWN OF TRUCKEE GENERAL PLAN DESIGNATIONS**  
 Figure 3.1-1

FIGURE 3-5: TOWN'S GENERAL PLAN DESIGNATIONS

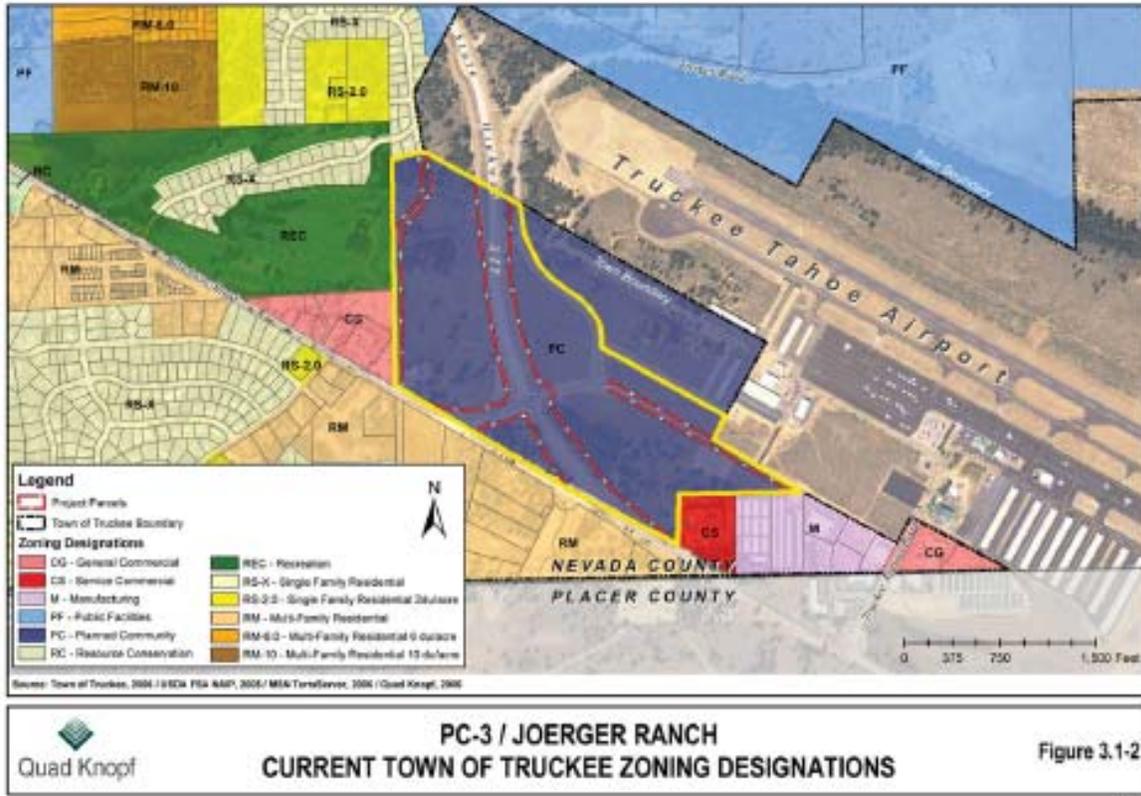


FIGURE 3-6: TOWN'S ZONING DESIGNATIONS

development is vital in accommodating the Town’s future growth while achieving the Town’s vision. Because of the size of the property, its development potential, its strategic location in the southern part of the Town and the need to coordinate development on a site divided by a state highway and major road, the Planned Community designation was applied to the Joerger Ranch property.

In addition to providing very general allowed land use and maximum development density (residential) and intensity (non-residential) guidance, the General Plan contains eight policies specific to PC-3. These are:

**Land Uses Allowed:** Land uses allowed in this designation are commercial, industrial, and high density residential uses.

**Density and Intensity:** An average FAR of 0.25 shall apply to commercial and industrial development. Residential uses are allowed at a maximum density of twelve housing units per acre.

**Policies:** The following policies will guide development of the Specific Plan for PC-3.

- **PC-3 – P1** Development allowed on the site will be a range of commercial, industrial and residential uses. Services for employees, such as day care facilities and food sales, shall be encouraged.
- **PC-3 – P2** The Specific Plan shall include design standards to provide for architectural consistency of development on the site, in accordance with the Town of Truckee design guidelines.
- **PC-3 – P3** Site design shall consider appropriate access to Highway 267, via Brockway Road and Soaring Way, and shall minimize visual impacts from the Highway 267 corridor.
- **PC-3 – P4** The Specific Plan shall include standards for the design of retail shopping areas that avoid “strip commercial” site layout, and that

- are oriented and scaled to the pedestrian realm.*
- **PC-3 – P5** *Specific Plan design standards shall include requirements for parking areas which promote attractive streetscapes, recognize the need for snow storage and removal, and reduce the visual impacts of large, unscreened parking lots through distributed landscaping, landscaped berms and other measures. Parking shall be provided in accordance with the Town of Truckee Design Guidelines.*
  - **PC-3 – P6** *The Specific Plan shall include provisions for supplying, onsite, the required housing for 50 percent of the very-low, low- and moderate-income workforce associated with development of the site. If land use or noise compatibility requirements of the Airport Land Use Compatibility Plan preclude or reduce the total amount of housing that can be developed on PC-3, required workforce housing may be permitted to be located off-site.*
  - **PC-3 – P7** *All development on PC-3 shall support community character goals and policies for the Brockway Road Corridor.*
  - **PC-3 – P8** *Ensure that the mix of land uses in the PC-3 Specific Plan will generate an amount of traffic that, in addition to buildout of the General Plan (considering all planned circulation improvements), would not result in the need for four lanes on Highway 267 between Interstate 80 and the Brockway Road/ Soaring Way intersection.*

## 5.2 Development Code

The Development Code, known as Title 18 of the Truckee Municipal Code, contains the Town's zoning, land use and subdivision regulations, representing the majority of the Town's requirements for the development and use of private and public land. The Development Code is supplemented by additional

requirements for construction and other aspects of development and land use contained within other Titles of the Truckee Municipal Code. The Joerger Ranch Specific Plan relies upon many of the standards and guidelines of the Development Code. Chapter 8 – Development Standards & Design Guidelines of the Plan contains a more specific description of the Plan's relationship with the Development Code, including a description of the Plan's reliance on existing standards and guidelines and those created specifically to suite development in the Plan Area. Figure 2-6 indicates the zoning designations in the vicinity of PC-3.

## 5.3 Town Trails & Bikeways Master Plan

The Truckee Trails & Bikeways Master Plan seeks to create a comprehensive system of trails and bikeways to promote both non-motorized alternative transportation and create additional recreation opportunities for the Truckee community and beyond. Although the scope of the Master Plan is limited to the Town's jurisdictional boundaries, the importance of providing connections to the larger Truckee-North Lake Tahoe is recognized and encouraged for the purpose of creating a seamless regional system.

Joerger Ranch provides an opportunity to implement key in-town trails, bikeways and connections to the larger regional trail system. The Plan Area is located along State Route 267 and Brockway Road/Soaring Road, providing an opportunity for important connective bikeway links along these highly-traveled public rights-of-way for the benefit of both the local community and visitors. The Plan Area is also located in close proximity to three of the Town's most-prominent recreational assets – the Truckee River/Legacy Trail, the Riverview Sports Park and the Truckee River Regional Park. Development of Joerger Ranch provides the opportunity to construct these trail connections for benefit of the community and Joerger Ranch residents and visitors.

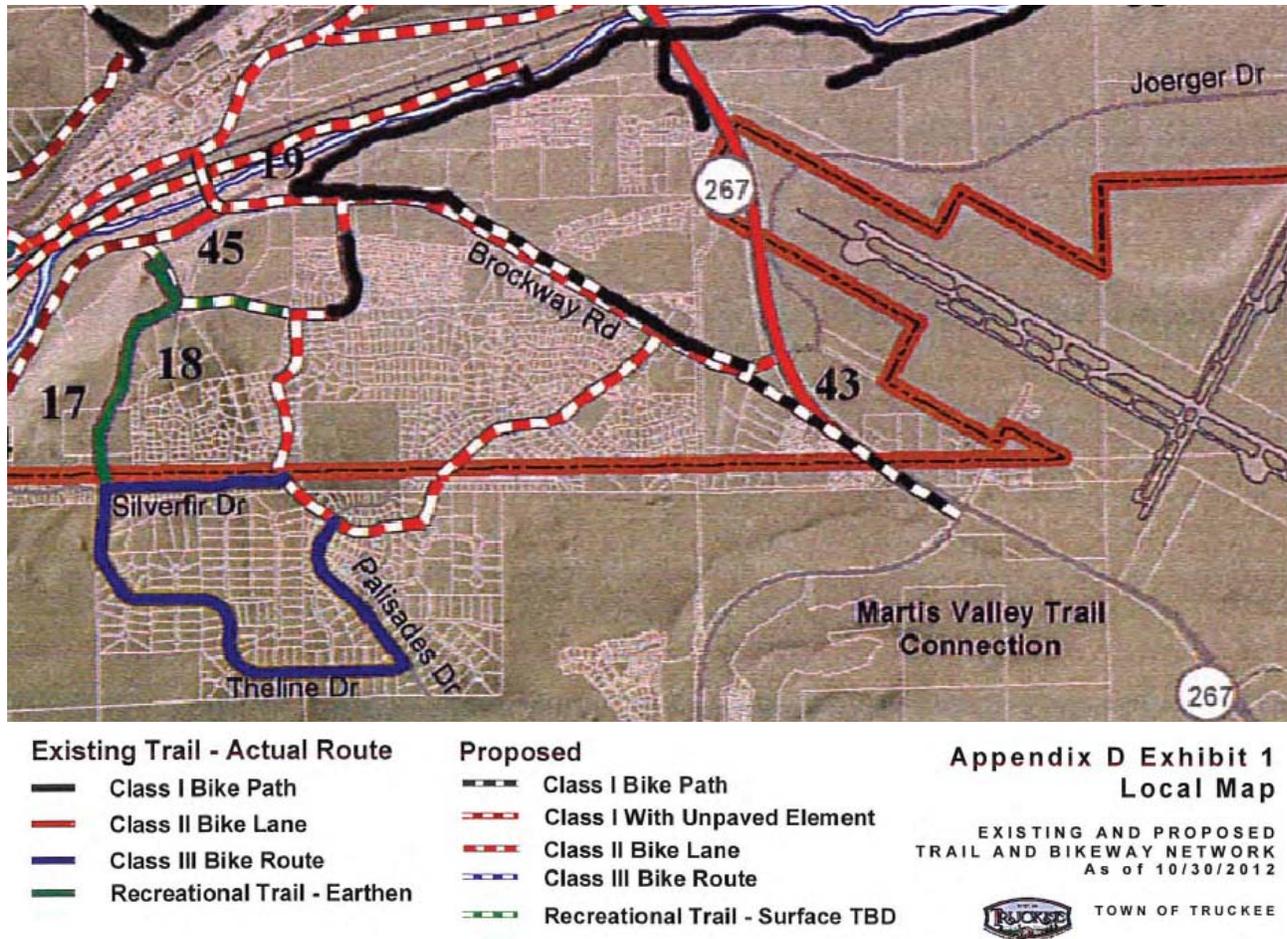


FIGURE 3-7: TOWN'S TRAIL AND BIKEWAY NETWORK

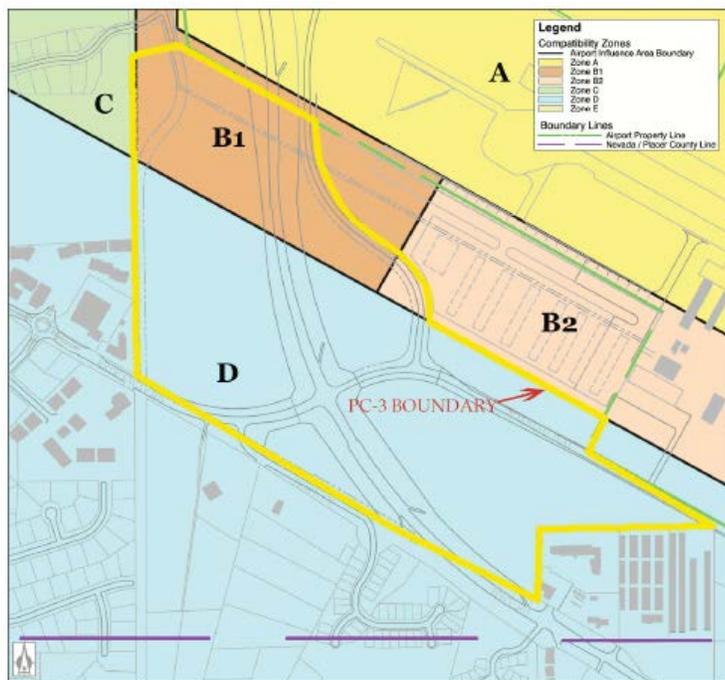


FIGURE 3-8: AIRPORT'S COMPREHENSIVE LAND USE PLAN

### 5.4 Tahoe Truckee Airport Land Use Compatibility Plan and Master Plan

#### COMPATIBILITY PLAN

The Truckee Tahoe Airport Land Use Compatibility Plan (CLUP) was adopted in 1986 and revised in 1990 and 2004 by the Foothill Airport Land Use Commission (FALUC). Under State Law, the FALUC must approve “the adoption or approval of any amendment to a general or specific plan affecting the property within an airport influence area.” The policies of the CLUP aim to promote land use compatibility with the Truckee Tahoe Airport, focused on four potential concerns — aircraft noise, land use safety (both people on the ground and

occupants of aircraft), protection of airport airspace and general concerns related to aircraft over flights. The CLUP imposes a separate and independent set of policies and restrictions on lands surrounding the airport than that of the local land use authority(s).

The area surrounding the Truckee Tahoe Airport is divided into different zones, each containing corresponding regulations for density, open space, height and land use. Generally there is close relationship between the proximity of the land area to the airport and the degree of restriction imposed upon land uses under the CLUP.

The Joerger Ranch Plan Area is primarily located with Compatibility Zone B-1 (Inner Approach/Departure) and Zone D (Primary Traffic Patterns). A small, approximately 8-acre portion of the northwest corner of the Plan Area is located within Zone B-2 (Adjacent to Runway). While the policies and concept plans contained within this Specific Plan have been developed to be consistent with each of these respective Compatibility Zones, it will be necessary to closely evaluate and insure consistency with the CLUP for this Specific Plan and future development proposals implementing the Specific Plan.

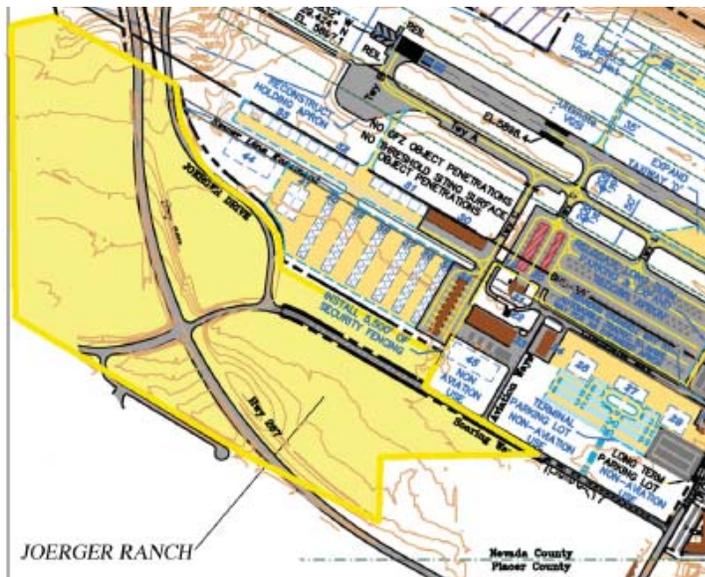


FIGURE 3-9: AIRPORT'S MASTER PLAN

## MASTER PLAN

The Truckee Tahoe Airport Master Plan was adopted in November 1998 by the Tahoe Truckee Airport District Board of Directors. The Airport Master Plan contains a forecast of aviation demand and facility needs for the airport through the year 2020. These forecasts were utilized to identify development alternatives which, in turn, were refined into a single master plan concept representing the recommended direction for future use and development of lands owned by the Truckee Tahoe Airport District.

The Airport Layout Drawing (Sheet 1 of 9) contained within the 1998 Airport Master Plan was last updated in 2005. This update included the identification of non-aviation use lands, largely following the land areas similarly-designated as “available for alternative land use” within the On-Airport Land Use Drawing (Sheet 8 of 9) of the 1998 Airport Master Plan. This is relevant in that the majority of these non-aviation use designated lands about the Joerger Ranch Plan Area to the north and east and all are currently undeveloped. Although the Airport Master Plan does not specifically define appropriate or envisioned uses for these non-aviation use lands, the ultimate development pattern of Joerger Ranch will influence

and guide their future development as a matter of compatibility and character, particularly along Soaring Way and the significant changes proposed for this roadway under this Specific Plan.

### 5.5 Martis Valley Community Plan (MVCP)

The relationship of the Martis Valley and the Martis Valley Community Plan (MVCP) to Joerger Ranch has been described, in part, in Section 2 of this Chapter. The MVCP designates the lands adjacent to the southerly Town boundary (and adjacent to the Joerger Ranch Plan Area) as General Commercial, a designation also applied to lands in and

around the Truckee Tahoe Airport, and to existing commercially-used lands on the north side of State Route 267 terminating near the intersection of Truckee Airport Road. A small area of land south of State Route 267 has been given the Professional Office designation within the MVCP. An Open Space land use designation is applied to the remaining lands along State Route 267 within the Martis Valley. The MVCP does not have regulatory authority over the Joerger Ranch Plan Area, however it does provide contextual information for the Plan Area.

**5.6 Placer & Nevada County General Plans**

Although the MVCP is the primary policy document for Placer County in the Martis Valley, it is a component of the broader Placer County General Plan adopted in 1994. Like the MVCP, the Placer County General Plan recognizes Placer County’s close relationship with Truckee and encourages coordination and communication between the Town of Truckee and Placer County for all land use and infrastructure improvements. Although the Placer

County General Plan does not maintain regulatory authority over the Joerger Ranch Plan Area, the Town of Truckee and Placer County maintain a cooperative working relationship. While the MVCP addresses specific community concerns and issues in the Martis Valley, it does not supersede the Placer County General Plan.

The Nevada County General Plan was adopted in 1995. Similar to Placer County’s relationship with the Town of Truckee, the Nevada County General Plan has no regulatory authority over the Plan Area, but the county and town also maintain a cooperative working relationship. Because the lands of Nevada County covered under this General Plan maintain little connectivity and/or adjacency with the Joerger Ranch Plan Area due to the location of the Tahoe Truckee Airport, the Nevada County General Plan has little relationship with the Joerger Ranch Specific Plan. The exception is the area on the north side of Soaring Way to the east of the Plan Area with an IND (Industrial) land use designation and a small

area north of the Plan Area on either side of State Route 267 with a CC (Community Commercial) land use designation. Both of these areas are within the ownership boundaries of the Tahoe Truckee Airport. The remaining airport land area is designated by the Nevada County General Plan as PUB (Public).

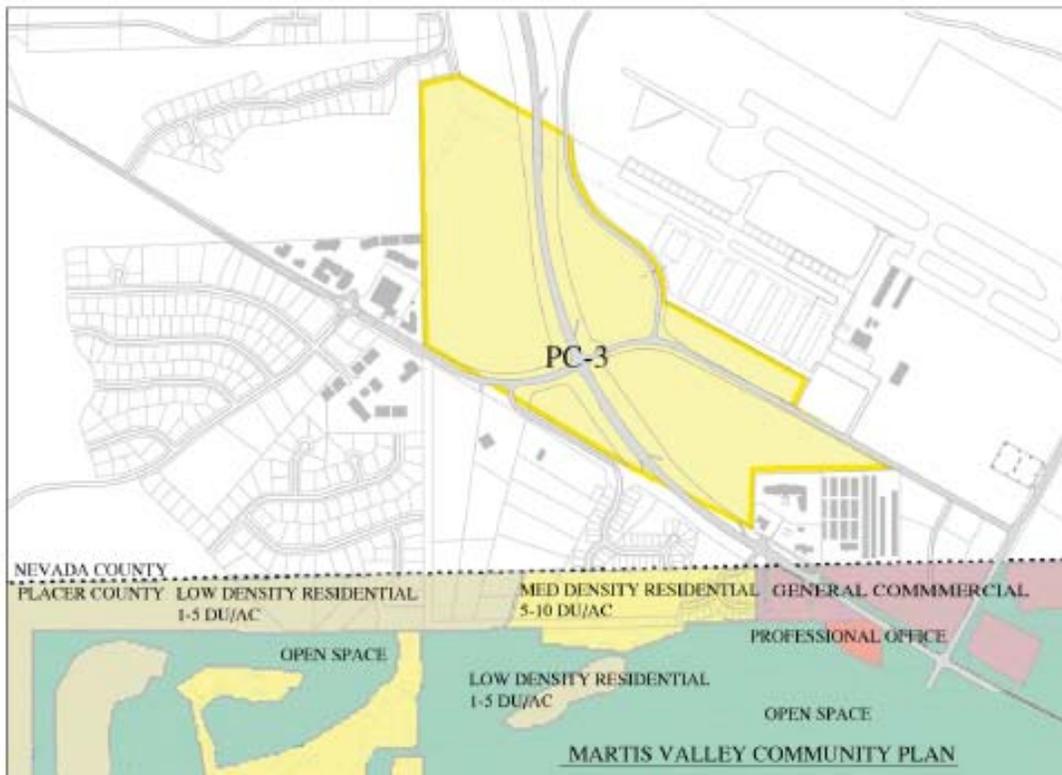


FIGURE 3-10: MARTIS VALLEY COMMUNITY PLAN

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# Chapter 4

## Land Use Standards





FIGURE 4-1: PC-3 PLAN AREA

## 1 | Introduction

To successfully implement General Plan principles of fostering and strengthening a balanced economy, this Specific Plan focuses on attracting and retaining businesses and commercial activities that build upon and enhance Truckee’s unique small mountain town character and natural environment.

This chapter includes the regulatory plan, allowable land uses, development standards, and design guidelines for each zoning district within the PC-3 plan area. Where regulations from the Development Code apply to future development of the Joerger Ranch Specific Plan, the regulations are incorporated in this plan either directly or by reference.

### TARGETED LAND USES

Targeted Land Uses are those that are considered “preferred” or “highly desirable”. Land uses within this category are identified as those that cannot be easily accommodated in other established areas within Town, significantly contribute to the character of the district, and/or support the respective economic

cluster concept. A variety of potential “Land Uses” are listed in Table 1.1A to provide flexibility for future development, however, “Targeted Land Uses” are preferred uses. To incentivize Targeted Land Uses, a Development Permit is **only** required for projects greater than 15,000 s.f. of gross floor area.

### DEVELOPMENT STANDARDS

Development Standards address aspects of development that are essential to achieve the goals of the Specific Plan. Any new land use and/or structure shall comply with the standards identified in this Specific Plan.

### DESIGN STANDARDS

Design Standards are intended as a guide to assist property owners and project designers in understanding the Town’s goals for attaining high quality development that is sensitive to the Town’s unique small mountain-town character. These standards provide guidance for new development related to aesthetics, character and design details and are intended to guide building and site design.

Whereas conformance with the development standards is mandatory, the review authority may interpret the design standards with some flexibility in the application to specific projects, as not all design criteria may be workable/appropriate for each project. In some circumstances, one standard may be relaxed to facilitate compliance with another standard determined by the review authority to be more important in the particular case. The overall objective is to ensure that the intent and spirit of the design standards are followed.

#### **LANDSCAPE STANDARDS & DESIGN GUIDELINES**

For Landscape Standards & Design Guidelines, refer to the Town of Truckee Development Code, Chapter 18.40 (Landscape Standards) and Chapter 18.42 (Landscape Design Guidelines).

## 2 | General Commercial (CG) Zoning Districts

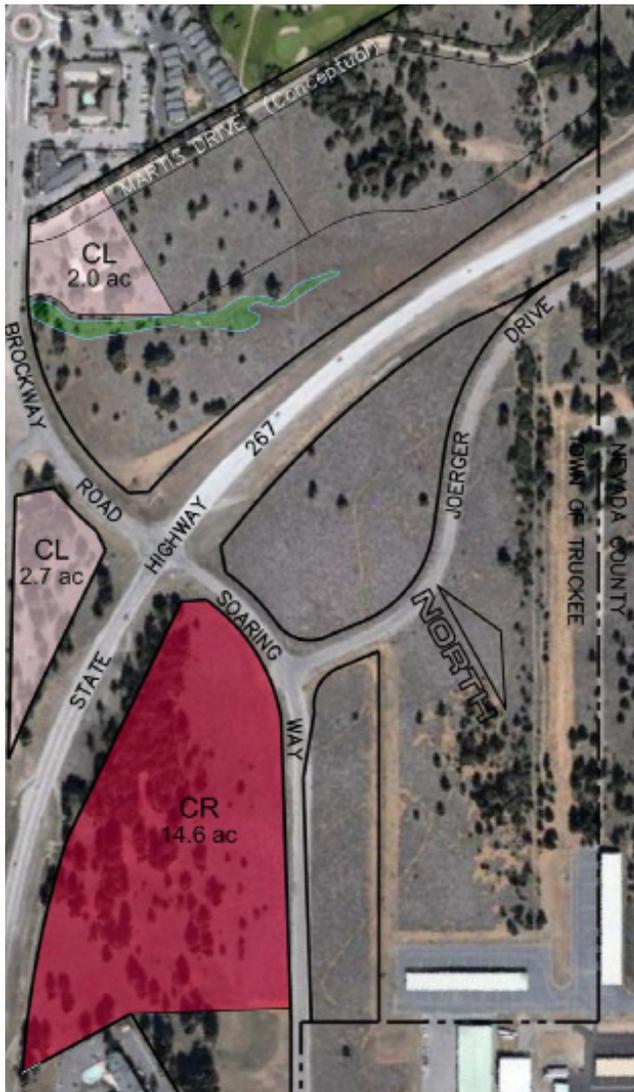


FIGURE 4-2: CG ZONING DISTRICT

### Purpose

Commercial zoning within the Joerger Ranch Specific Plan area is applied to areas appropriate for both local and regional-serving uses including larger floor plate uses not easily accommodated in other established commercial-zoned areas. The intent is to encourage locally operated businesses and land uses that can enhance Truckee's unique small town mountain atmosphere. Typical national retail chains are

discouraged unless it can be demonstrated through an economic analysis that it would benefit the Town as a whole while still maintaining Truckee's unique small town mountain atmosphere.

The General Commercial (CG) zoning districts include:

- CL - Lifestyle Commercial
- CR - Regional Commercial

### General Development Standards

**CG-1:** Loading facilities shall be located at the rear of buildings and screened from any public rights-of-way with fencing and/or landscaping.

**CG-2:** Any land uses established prior to the date of the adoption of this Specific Plan and located in the River Protection (-RP) or Historic Preservation (-HP) overlay districts shall be considered a Targeted Land Use.

**CG-3:** Parking lots with 50 or more parking spaces shall be divided into separate areas by landscaping or walkways at least ten feet in width, or by a building or group of buildings.

**CG-4:** The construction and operation of solid waste & recyclable material storage areas shall comply with Development Code Section 18.30.150 (Solid Waste / Recyclable Materials Storage).

**CG-5:** Maintain a minimum 30' building setback from Highway 267 right-of-way. Preserve tree clusters within this setback area and prohibit tree removal of greater than 12" diameter.

**CG-6:** All land use applications for development on the CR (Regional Commercial) zoned parcel shall include a tree preservation plan for review and approval by the Planning Commission. The plan shall demonstrate that the proposed tree

protection and removal will ensure the preservation of a sufficient amount of mature trees to screen buildings from views along Highway 267.

**CG-7:** Prohibit the use of beige, tan, cream, white and similar colored stucco and plaster.

**CG-8:** Woodstoves, zero clearance fireplaces, and other woodburning devices are prohibited. Wood-fired ovens may be approved by the Community Development Director with payment of air quality mitigation fees.

**CG-9:** Commercial and/or industrial uses shall be the primary components of a mixed-use project. Residential floor area shall not exceed 50 percent of the total floor area of the mixed-use project. For example, a project with 5,000 square feet of industrial warehouse space may have up to 5,000 square feet of residential floor area. The commercial and manufacturing/industrial uses within a mixed-use project be open to the general public, rather than being restricted to tenants of the project. Services such as a laundromat, child care center or playground that are primarily restricted to serving the building residents must be identified and included as part of that residential project).

**CG-10:** Mixed-use projects and standalone residential projects are required to comply with Development Code Section 18.58.180 (Multi-Family Residential Projects) and with the RMW-20 Design Standards.

**CG-11:** In mixed-use projects and standalone residential projects, buildings and common open space shall be located and oriented to maximize solar exposure and passive heating.

**CG-12:** In mixed-use projects and standalone residential projects, parking shall include a minimum of two electric vehicle charging stations. These stations may be combined with guest parking.

**CG-13:** The minimum number of bicycle parking spaces shall equal 25 percent of the parking demand

for the project. At least 45 percent of the required bicycle parking spaces shall be provided as enclosed bicycle parking spaces, such as bicycle lockers. At minimum, the number of bicycle parking spaces provided within 50 feet of each mixed-use building or residential building shall be equivalent to the number of residential units included in each building.

### General Design Standards

**CG-G1:** A variety of building sizes, heights, and orientation shall be provided. Physical and visual connections shall be provided between adjacent buildings and projects in adjacent on- and off-site development.

### Pedestrian Access and Circulation

**CG-G2:** Site designs shall provide pedestrian access from the public street(s) to the main entrance of each building.

**CG-G3:** Separated sidewalks are required within parking lots.

**CG-G4:** Each building shall have a clearly defined pedestrian access route connecting adjacent buildings throughout the project.

### Plazas and Paving

**CG-G5:** Buildings shall be arranged to create “outdoor rooms” or plazas. The location and purpose of plazas shall be carefully considered to ensure they are functional.



Plazas & paving patterns should promote places where people can gather.

**CG-G6:** Exterior spaces shall provide shade during the summer and protection from wind, rain and snow in the winter. North-facing plazas shall be avoided.

**CG-G7:** Hardscape shall address color, pattern, durability and function.

**CG-G8:** Permeable or interlocking pavers shall be utilized in heavily traveled pedestrian areas such as primary walkways and plazas.

### Architectural Design

**CG-G9:** Buildings shall be designed to maximize solar gain and passive heating, as well as being adaptable to a variety of uses and tenants throughout the life of the building.



Example of Appropriate CG Architecture

**CG-G10:** Development shall incorporate mature trees to the extent feasible.

**CG-G11:** Imaginative use of traditional or contemporary styles shall be used to suggest a style from Truckee's past without directly copying a style.

**CG-G12:** Building heights shall relate to plazas and gathering areas, allowing for maximum sun and ventilation, protection from the elements and enhanced views of surrounding natural features.

**CG-G13:** Building materials, forms and details shall reflect Truckee's small town mountain character.

### Forms and Massing

**CG-G14:** Buildings within the CG Zoning District shall be designed to incorporate a combination of 1- and 2-story design elements to reduce visual massing while accommodating allowed residential densities in mixed-use projects, anchored by vertical elements with interesting roof lines.

**CG-G15:** All building elevations shall be articulated with details and elements from the primary façade. Blank, unarticulated building façades shall be avoided.

### Scale

Buildings greater than 5,000 s.f. shall incorporate the following:

**CG-G16:** Vary the building heights along a street to achieve more visual interest and distinct building character, avoiding the monotony of continuous building height.

**CG-G17:** Combine horizontal and vertical elements to provide variation, interest and rhythm within and among buildings.

**CG-G18:** Articulate the different parts of a building façade through arrangement of façade elements and/or a change in materials.

**CG-G19:** Break up large buildings by changes in wall planes, eaves, windows or other architectural ornamentation as well as thoughtful combinations of complementary material changes.

### Entries

**CG-G20:** Building frontages shall have inviting entries that create a welcoming atmosphere. Entries shall be prominently placed for easy access from the public right-of-way.

**CG-G21:** Design buildings on corner lots with two front entries facing each public right-of-way.

**CG-G22:** All building elevations facing and/or adjacent to Highway 267 shall be well articulated

and shall include one or more of the following features to enhance the building's aesthetics.

- 25% of the wall surface area to window openings.
- Use of window and/or entries that reinforce the pattern of small storefront spaces.



Inviting entries with easy access are encouraged.

**Building Materials**

**CG-G23:** All building materials shall be durable and of high quality such as:

- a. **STONE** Stone such as weathered granite, basaltic rock, or natural stone shall be used to tie buildings to their sites and may also be used to express structural mass in walls and chimneys. Stone walls shall appear load-bearing, as opposed to a thin veneer.



- b. **BRICK** Brick is functional and creates texture.
- c. **CONCRETE WALLS** Concrete walls with formed, fluted details and staining to provide an aesthetically rustic look for exterior walls; board-form concrete.

- d. **WOOD BOARD AND BATTEN** Vertical board and batten, as well as reversed wide battens over boards or rough sawn plywood with batten in various sizes and widths.



- e. **WOOD SIDING** Various sizes and profiles of

wood siding in horizontal or vertical patterns. Stain, semi-transparent stain and painted wood are permitted.

- f. **METAL SIDING** Metal siding as an alternative to wood siding. When used, metal siding shall be allowed to naturally weather and blend with the subtle earth tones of the site (i. e. corten steel and similar metals).



- g. **ORNAMENTAL AND STRUCTURAL STEEL** Steel materials as accent elements to reinforce the structural expression and crafted nature of the building. Appropriate uses include exposed structural numbers, metal banding at column bases, steel cross-ties and steel connectors at timber connections.

**Windows**

**CG-G24:** Window patterns shall reflect Truckee's utilitarian design character.

**CG-G25:** Aluminum storefront windows are permitted for retail spaces.

**Roofs, Gutters & Downspouts**

**CG-G26:** Roofs shall be designed to adequately address snow shedding, and include details such as ventilation, insulation, gutters, etc. The roof design shall avoid conflicts with drainage, safety issues and landscaping.

**CG-G27:** The use of snow cleats is not permitted. Roof design shall consider snow conditions appropriately to avoid the need to add snow retention devices.

**CG-G28:** Mechanical equipment shall be integrated within the roof design and not visible from the roadway.

**CG-G29:** Gutters, downspouts, splitters and flashing

shall be fabricated from metal and coating shall be integrated in a manner that relates to or matches the primary or secondary roof color.

#### Colors

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**CG-G30:** Building colors shall be drawn from the natural, surrounding environment. The use of light tans, beige and similar corporate shopping center palette is prohibited.

**CG-G31:** Minimize the number of colors appearing on a single building exterior.

**CG-G32:** Accent colors for trim shall be limited in number and used sparingly. Accent colors on adjacent buildings shall be chosen to complement one another.

#### Exterior Equipment

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**CG-G33:** All exterior mechanical, electrical and other utility equipment (such as metering devices, transformers, gas meters and satellite dishes) shall be substantially screened from public view and adjacent buildings. Wall-mounted utilities shall be screened using landscaping and/or material similar to the exterior walls, with exposure only as required by utility companies for meter reading.

#### Photovoltaic Panels and Shingles

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**CG-G34:** Photovoltaic panels, shingles and other solar devices are allowed on roofs if incorporated as part of the roof or roof form.

**Table 4.1A** identifies the uses of land allowed by this Specific Plan in the commercial zoning districts, and the land use permit required to establish each use. Refer to the Development Code for definitions of land uses that are not specifically defined in this Specific Plan.

LAND USES (1)	PERMIT REQUIREMENT BY ZONING DISTRICT	
	CL	CR
<b>PROCESSING USES</b>		
Beverage Production & Food Production	P	UP
Food & Beverage Distribution	P(2)	P
Recycling - Reverse Vending Machines	NP	P
Recycling - Small Collection Facility	MUP	MUP
<b>RECREATION, EDUCATION &amp; PUBLIC ASSEMBLY USES</b>		
Health / Fitness Facilities	P	P
Indoor Recreation Centers	P	P
Membership Organization Facilities	P	P
Outdoor commercial recreation	T	NP
Parks & Playgrounds	MUP	MUP
Schools - Public & Private	UP	UP
Schools - Specialized Education & Training	MUP	P
<b>RESIDENTIAL USES</b>		
Cohousing, in a commercial/industrial project	P	P
Live / Work Units	P	P
Multi-family Dwellings, 11 and more units	NP	NP
Multi-family Dwellings, in a commercial/industrial project	P	P
Work / Live Units	P	P
Senior Housing, in a commercial/industrial project	P	P
<b>RETAIL TRADE USES</b>		
Accessory Retail Uses	P	P
Alcoholic beverage sales, other than beer and wine	MUP	MUP
Auto Parts Sales	NP	P
Bars & Drinking Establishments	UP	UP
Convenience Stores	NP	UP
Furniture, Furnishings, & Equipment Stores	P	T
Grocery Stores or Specialty Market	NP	P

LAND USES (1)	PERMIT REQUIREMENT BY ZONING DISTRICT	
	CL	CR
Outdoor Dining & Seating Areas	T	P
Outdoor Sales & Displays of Merchandise	T	MUP
Plant Nurseries & Garden Supply Stores	T	P
Restaurants, Fast Casual	P	P
Restaurants Table Service	P	P
Retail Stores, General Merchandise	P*	P
Retail Stores, General Merchandise - Large	P	T
<b>SERVICE USES</b>		
Automated Teller Machines (ATMs)	NP	P
Banks & Financial Services	NP	P
Business Support Services	NP	P
Car Wash	NP	UP
Laundromat	NP	P
Lodging / Hotel	NP	P
Non-profit Community Building	T*	NP
Offices, accessory to primary use	P	P
Offices, business & professional - Large	P	P
Offices, business & professional	P*	P
Personal Services	P	P
Public Buildings and Structures	UP (3)	UP (3)
Public Safety and Utility Facilities	UP	UP
Research & Development (R&D)	P	P
Service Stations	NP	UP
Veterinary Clinics, Animal Hospitals, Kennels, Boarding	UP	UP
Visitor Center	P	P
<b>TEMPORARY USES</b>		
Commercial Filming	TUP	TUP
Off-site Contractor's Construction Yard	TUP	TUP
Offices, temporary	TUP	TUP
On-site Material Processing	TUP	TUP
On-site Soil Remediation Activities	TUP	TUP
Outdoor Retail Sales, temporary	TUP	TUP

Seasonal Sales Lot	TUP	TUP
Temporary Events, Non-profit Organization	TUP	TUP
Temporary Work Trailers	TUP	TUP
<b>TRANSPORTATION, INFRASTRUCTURE &amp; COMMUNICATIONS USES</b>		
Broadcasting Studios	P	P
Electrical Utility Facilities	UP	UP
Pipelines	UP	UP
Telecommunications Facilities	UP	UP
Transit Stations & Terminals	UP	UP
Transit Stops Shelters	P	P
Utility Lines	P	P

### Key to Permit Requirements

SYMBOL	PERMIT REQUIREMENT
P	Permitted use. Zoning Clearance required for projects with less than 7,500 sq. ft. of floor area and less than 26,000 sq. ft. of site disturbance.
DP	Development Permit required for permitted projects with 7,500 sq. ft. or more of floor Area or 26,000 sq. ft. or more of site disturbance
NP	Not permitted.
UP	Conditional use. Use Permit approval required
MUP	Conditional use. Minor Use Permit approval required
TUP	Temporary Use Permit approval required
T	Targeted Land Use (preferred use). Permitted. Development permit required ONLY for projects greater than 15,000 s.f. of floor area or 20 or more residential units; zoning clearance for less than 15,000 s.f. or less than 20 residential units.

**Notes:**

1. Definitions of the listed land uses are in Section 7 (Definitions, Glossary).
- (2) Ancillary distribution of products that are produced or stored on site is permitted, provided that such use does not generate more than 10 trips per day for vehicles less than 10,000 lbs.
- (3) Permitted if the public agency use (e.g. office, outdoor storage, etc.) is permitted in zoning district; UP otherwise.

\* Identified Land Uses specific to the CL-1 site located on Hope Court. This site has been offered for a local non-profit community center. If the non-profit group elects not to pursue development on this site, limited office/retail land uses may be pursued.

## General Development Standards

The Joerger Ranch Project shall be developed in accordance with the approved Overall Development Plan and consistent with the Town of Truckee CG Development Code standards, with the exception of those being modified by this Specific Plan, as shown on the applicable exhibits and as provided in the following table:

<b>(CG) GENERAL DEVELOPMENT STANDARDS</b>		
	<b>CL</b>	<b>CR</b>
Front Setback	Minimum: 0-foot if no parking is proposed between the building and the Town right-of-way; 10-foot landscaped setback when parking is proposed.	
Side Setback	Minimum: 0-foot; 6-foot landscaped setback when parking is proposed	
Rear Setback	No minimum except as may be required by other standards and/or guidelines contained within this Specific Plan.	
Floor Area Ratio (FAR)	0.25 Maximum	0.25 Maximum
Height Limit	50 feet	50 feet
Parking & Loading	As required by Development Code Chapter 18.48 (Parking and Loading Standards)	
Site Coverage	No maximum	
Landscaping	As required by Development Code Chapter 18.40	
Outdoor Activities	As allowed by Development Code Chapters 18.58.190 & 18.58.200	

## 2.1 | CL - Lifestyle Commercial



FIGURE 4-2.1: CL ZONING DISTRICT

### Purpose

The CL or “lifestyle” zone is applied to approximately 4.7 acres at the northeastern end of Brockway Road and at the intersection of Hope Court and Brockway Road. The underlying cluster concept for this zone groups uses that reflect Truckee’s outdoor lifestyle and desire for social connectivity. The CL zone is applied to the two parcels described below and is appropriate for businesses that promote the varied and interesting mix of land uses and architecture that currently exist within the Brockway Road Corridor. Desired uses include unique locally-owned and operated retail, service, non-profit and recreation uses, particularly those with outdoor display and/or activity.

**CL Parcel** - 2.0-acre parcel at the northeastern end of Brockway Road. This parcel is developed with a legal non-conforming, locally-owned and operated, winery with outdoor recreational amenities including bocce courts and barbeques. The continued use of the winery is permitted and encouraged and new uses proposed within

this zone should be compatible with the winery operations.

**CL-1 Parcel** - 2.7-acre triangular-shaped parcel located at the intersection of Hope Court & Brockway Road. This parcel is currently undeveloped. Its gateway location provides the opportunity to create a welcoming entrance to the Brockway Road Corridor and Downtown. Current challenges with this site are its proximity to existing residential uses and major regional roadways, an irregular shape, and a steep drop-off in topography to the northeastern edge. To implement Specific Plan goals and recognizing the site constraints, development potential of the CL-2 parcel is limited. The Targeted Land Use is a “non-profit center.” This site has been offered to a local non-profit community group for development of a community center. Should the non-profit group elect not to pursue development of a community center at this site, other land uses may be considered that promote Truckee’s outdoor lifestyle.

Although both of these parcels are based on the clustering of Truckee lifestyle uses, due to the uniqueness of each parcel, this Specific Plan identifies separate allowed land uses as well as development standards and guidelines for each parcel.

### Target Uses

- Home furnishings / furniture with indoor and outdoor displays
- Health, fitness, wellness
- Outdoor commercial recreation
- Casual dining restaurants with outdoor dining
- Wine tasting facilities & beverage garden
- Retail with outdoor sales & displays

- Outdoor gear sales & rentals
- Garden supplies & nursery sales
- *\*Community-serving uses (ie. non-profit)*

*\*Denotes CL-1 Land Use ONLY*



Examples of Outdoor Recreation Businesses

**CL-G4:** New buildings shall be located near Brockway Road similar to the buildings to the west of the site and provide sufficient area to accommodate outdoor display areas between the bike trail and the buildings.



Example of a Beverage Garden

**CL Development Standards**

**CL-1:** A 10-foot wide Class I bike path shall be provided along the frontage of Brockway Road between Mountain Home Center and Highway 267.

**CL Design Standards**

**CL-G1:** Preserve and enhance the open character and qualities of the Brockway Road corridor by including appropriate buffering and screening from the road corridor, and interspersing development areas or nodes with green space. (General Plan Policy P-14.1)

**CL-G2:** Ensure that the design quality and character of the PC-3 development is compatible with the gateway qualities of the south end of Brockway Road. (General Plan Policy P-14.3)

**CL-G3:** Land uses within the CL zoning district shall provide outdoor retail sales area, outdoor display, outdoor dining, or public gathering space (not applicable to CL-1 Parcel).

2.2 | CR - Regional Commercial



FIGURE 4-2.2: CR ZONING DISTRICT

**Purpose**

The CR zoning district is applied to an 16.3 acre site fronting Soaring Way and abutting State Highway 267 appropriate for large floorplate commercial and office uses that serve both local and regional markets, including limited restaurant and specialty retail uses. The targeted uses for the CR zoning district include grocery and/or specialty food markets; and large floor plate commercial, office and retail businesses with building footprints 5000 S.F. or larger. National retail chains are discouraged unless it can be demonstrated through an economic analysis that such a sue will benefit the Town as a whole and avoid or minimize economic impacts to Downtown (See Policy P7.3).

**Target Uses**

- Grocery market
- Specialty food market
- General merchandise - Large floor plate
- Home furnishings & appliances - Large floor plate
- Apparel - Large floor plate
- Office - Large floor plate
- Casual dining restaurants



Well Articulated Large-Floor Plate Building Design



Example of Casual Dining Restaurant

**CR Development Standards**

- CR-1:** Parking lots shall be limited to 40% of a parcel’s street frontage.
- CR-2:** A minimum 6-foot landscape buffer shall be provided adjacent to the edge of the existing or future sidewalk along Soaring Way.
- CR-3:** Planned Development approval shall be required in conjunction with or prior to subdivision of any property within the CR zone. The purpose of this requirement is to ensure that development of the entire zone is considered comprehensively.
- CR-4:** Parking lots shall be prohibited within 100-feet of the State Highway 267 right-of-way boundary.

**CR-5:** Parking lots located within 300-feet of the State Highway 267 right-of-way boundary shall provide screening equivalent to two trees and four shrubs for every five parking stalls. Where the review authority determines that the parking is sufficiently screened by buildings, this requirement may be lessened.

### **CR Design Standards**

**CG-G1:** Connectivity shall be provided between all buildings. For pedestrian connections that cross parking areas, stained or stamped concrete, pavers, or similar treatment shall be used to delineate the travel path.

**CG-G2:** Buildings proposed along the Soaring Way frontage shall provide a primary entry facing Soaring Way and a primary entry facing the main parking area. Primary entries consist of prominent architectural and site features including but not limited to promenades, seating, decorative pedestrian paths and/or plazas, prominent landscaping and/or public art.

**CG-G3:** Buildings shall be sized in relationship to each other to create a convenient block with visibility to portions of the each building from the adjacent roadways.

**CG-G4:** Parking areas shall be screened and retain existing tree clusters, where possible.

**CG-G5:** Provide a Class I trail within the Open Space that fronts State Highway 267 between Soaring Way and the lodging use to the southeast.

**CG-G6:** Incorporate the existing trees along State Highway 267 into the parking area landscape design to form a buffer zone that allows for filtered views from the roadway to the Center.

**CR-G7:** Building orientation shall maximize solar gain and passive heating, and be considered with respect to views from State Highway 267 and Soaring Way.

### 3 | Manufacturing / Industrial & Business Innovation Zoning Districts



FIGURE 4-3: M ZONING DISTRICT

**Purpose**

The intent of M Zoning District in the Joerger Ranch Specific Plan is to attract new manufacturing and industrial businesses and provide opportunities for relocation of Truckee River corridor businesses. The primary goal of the M district is to promote economic diversification and land use opportunities that can strengthen Truckee’s economic base by encouraging growth in green industry, manufacturing, research and development, high tech industry, processing & distribution. The M District also is intended to encourage relocation of industrial and manufacturing uses from the Truckee River Corridor.

The M District within the Joerger Ranch Specific Plan is divided into two sub-zones to better promote the District’s purpose: (1) “Business Innovation Zone” (BIZ) for the land area west of State Highway 267 and (2) “Manufacturing/Industrial” (M1) for that land area east of State Highway 267.

The BIZ zoning is designed to attract businesses that provide Research and Development, production of local products (custom wood furniture, specialty food/spirit products), green construction design and material production and other similar uses. This zone would support more light industrial uses in a campus setting in support of the business innovation economic cluster.

The M1 zoning along Soaring Way is intended to attract a variety of manufacturing/industrial uses. Uses such as warehousing space, specialty food production, manufacturing, lumber yard, home improvement center and auto repair are allowed

within this zone.

Both the BIZ and M1 zoning districts have General Development Standards and General Design Guidelines to guide new development within each designation. These General Development Standards and Design Guidelines are intended to be implemented for the purpose of creating a relatively consistent theme within the Plan Area. Each zoning district also has specific standards and guidelines to encourage economic diversity and expansion and promote specific goals within each zone. If, through implementation of the Plan, there is a conflict between standards and guidelines, the specific standards and guidelines shall take precedence. The following are General Development Standards and Design Guidelines applicable to both the BIZ and M1 zoning districts, followed by the standards and guidelines specific to each sub-zone:

#### **General Development Standards**

**M-1:** No blank, unarticulated building facades are permitted.

**M-2:** Loading and service areas for delivery of merchandise should be situated away from storefronts and pedestrian areas and screened from State Highway 267 and shall be in accordance with Chapter 18.48 (Parking and Loading Standards) of the Development Code.

**M-3:** The construction and operation of solid waste & recyclable materials storage areas shall comply with Development Code Section 18.30.150 (Solid Waste / Recyclable Materials Storage). Refuse containers shall be placed within “bear-resistant” enclosures designed to be compatible with the project.

**M-4:** Mechanical equipment shall be integrated within the roof design and not visible from the roadway.

**M-5:** Woodstoves, zero clearance fireplaces, and other woodburning devices are prohibited. The Community Development Director may approve wood-fired ovens (one per business maximum) for restaurant use with payment of air quality mitigation fee.

**M-6:** Commercial and/or industrial uses shall be the primary components of a mixed-use project. Residential floor area shall not exceed 50 percent of the total floor area of the mixed-use project. For example, a project with 5,000 square feet of industrial warehouse space may have up to 5,000 square feet of residential floor area. The commercial and manufacturing/industrial uses within a mixed-use project must be open to the general public, rather than being restricted to tenants of the project. Services such as a laundromat, child care center or playground that are primarily restricted to serving the building residents must be identified and included as part of the residential project.

**M-7:** Mixed-use projects are required to comply with the RMW-20 Design Standards.

**M-8:** In mixed-use projects, buildings and common open space shall be located and oriented to maximize solar exposure and passive heating.

**M-9:** In mixed-use projects, parking shall include a minimum of two electric vehicle charging stations. These stations may be combined with guest parking.

**M-10:** The minimum number of bicycle parking spaces shall equal 25 percent of the parking demand for the project. At least 45 percent of the required bicycle parking spaces shall be provided as enclosed bicycle parking spaces, such as bicycle lockers. At minimum, the number of bicycle parking spaces provided within 50 feet of each mixed-use building or residential building shall be equivalent to the number of residential units included in each building.

### General Design Standards

**M-G1:** Service areas at the rear of buildings shall be consolidated in order to minimize pedestrian conflicts.

**M-G2:** Green Building practices are required. Construction of the buildings and related infrastructure shall promote healthy, efficient building practices. See Section 3.8 for more specifics.

**M-G3:** Preserve where feasible, existing mature trees.

**M-G4:** Buildings shall be designed to accommodate changes in uses and tenants over time.

**M-G5:** Buildings shall be located and oriented to provide maximum solar gain and passive heating.

### Plazas and Paving

**M-G6:** Exterior spaces shall provide shade during the summer and protection from wind, rain and snow in the winter.

### Architectural Design

**M-G7:** Building materials, forms and details shall create a relationship with the surrounding area and reflect Truckee's small town mountain character.

**M-G8:** The buildings shall incorporate a combination of 1- and 2-story massing, anchored by vertical elements with a variety of roof forms.

**M-G9:** Special architectural attention shall be placed on exterior building elevations that front on State Highway 267.

**M-G10:** Building materials and simple building forms that reflect Truckee's utilitarian design are permitted, including materials that appear weathered over time, such as rough saw wooden siding, scoured concrete, and corrugated metal.

### Scale

Large, bulky buildings shall be enhanced through

one or more of the following methods:

**M-G11:** Vary the planes of exterior walls in depth and/or direction.

**M-G12:** Vary the height of the buildings so they appear to be divided into distinct massing elements.

**M-G13:** Articulate the different parts of a building's façade through the use of color, the arrangement of façade elements and/or a change in materials.

**M-G14:** Break up large, dominating buildings with landscape materials, awnings, eaves, windows or other architectural ornamentation along with combinations of complementary colors.

**M-G15:** Use architectural detailing at the ground level to lessen the impact of an otherwise bulky building.

### Entries

**M-G16:** Buildings on corner lots shall be designed with two front entries facing the public rights-of-way.

**M-G17:** Access to second-story space shall be provided by a stair entry located through a separate entrance.

### Building Materials

**M-G19:** All building materials shall be durable and of high quality (see "CG" Zoning District for examples):

- a. **STONE** Stone such as weathered granite, basaltic rock, or natural stone to tie buildings to their sites and express structural mass in walls and chimneys. Stone walls shall appear load-bearing, as opposed to a thin veneer.
- b. **BRICK** Brick is functional and creates texture.
- c. **CONCRETE WALLS** Concrete walls with formed, fluted details and staining, and board-form concrete.
- d. **WOOD BOARD AND BATTEN** Vertical board

and batten, as well as reversed wide battens over boards or rough sawn plywood with batten in various sizes and widths.

- e. **WOOD SIDING** Various sizes and profiles of wood siding in horizontal or vertical patterns. Stain, semi-transparent stain and painted wood are permitted.
- f. **METAL SIDING** Metal siding as an alternative to wood siding. When used, metal siding shall be allowed to naturally weather and blend with the subtle earth tones of the site (i.e., corten steel and similar metals).
- g. **ORNAMENTAL AND STRUCTURAL STEEL** Steel materials used as accent elements to reinforce the structural expression and crafted nature of the building. Appropriate uses include exposed structural numbers, metal banding at column bases, steel cross-ties and steel connectors at timber connections.

#### Doors and Windows

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**M-G20:** Bay doors shall be designed to be compatible with the architectural character of the building.

**M-G21:** Glass shall be clear and have low reflectance with a high performing coating, such as “Low E”.

**M-G22:** Aluminum storefront windows shall be painted, not galvanized. Colors shall be compatible with the surrounding materials and warm in tone or black.

#### Roofs

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**M-G23:** Roofs shall be designed to accommodate snow shedding, and include details such as ventilation, insulation, gutters, etc. The roof design shall also avoid conflicts with drainage and safety issues and landscaping.

**M-G24:** Roof overhangs shall be used to shade large glass areas and avoid reflective glare. Colored and reflective glass is not permitted.

#### Gutters and Downspouts

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**M-G25:** Gutters and downspouts shall be designed to effectively divert water away from entries.

**M-G26:** Gutters, downspouts, splitters and flashing shall be fabricated from metal and coating shall be integrated in a manner that relates to or matches the primary or secondary roof color.

#### Colors

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**M-G27:** Color guidelines within the M Zoning District(s) are identical to those listed in the CG Zoning District.

#### Exterior Equipment

---

**M-G28:** All exterior mechanical, electrical and other utility equipment, such as metering devices, transformers, gas meters and satellite dishes, shall be substantially screened from public view and adjacent homes. Wall-mounted utilities shall be screened using landscaping or material similar to the exterior walls, with exposure only as required by utility companies for meter reading.

#### Photovoltaic Panels and Shingles

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**M-G29:** Photovoltaic panels and shingles placed on roofs shall be incorporated as part of the roof or the roof form.

**Table 4.2A** identifies the uses of land allowed by this Specific Plan in the manufacturing districts, and the land use permit required to establish each use.

LAND USES (1)	M1	BIZ
<b>MANUFACTURING, INDUSTRIAL &amp; PROCESSING USES</b>		
Beverage & Food Production & Distribution	P	T
Clothing Products	P	UP
Furniture & Fixtures Manufacturing	P	P
Glass Products	P	P
Handcraft Industries	P	P
Lumber & Wood Products	T	NP
Machinery Manufacturing	P	NP
Metal Fabrication, Machine & Welding Shops	T	UP
Pharmaceuticals	P	NP
Printing & Publishing	P	P
Stone & Cut Stone Products	P	NP
Structural Clay & Pottery Products	P	NP
Warehousing, Wholesale & Distribution	T	P
Green Technology	T	T
<b>RECREATION, EDUCATION &amp; PUBLIC ASSEMBLY USES</b>		
Churches / Places of Worship	NP	NP
Community Centers	NP	NP
Docks and Piers, commercial	NP	NP
Health & Fitness Facilities	P	P
Parks & Playgrounds	NP	MUP
Schools - Specialized Education & Training	NP	MUP
<b>RESIDENTIAL USES</b>		
Cohousing, in a commercial/industrial project	P	P
Live / Work Units	P	P
Multi-family Dwellings, in a commercial/industrial project	P	P

LAND USES (1)	M1	BIZ
Work / Live Units	P	P
Senior Housing, in a commercial/industrial project	P	P
<b>RETAIL TRADE USES</b>		
Accessory Retail Uses	P	P
Alcoholic beverage sales, other than beer and wine	MUP	MUP
Auto Parts Sales	P	NP
Building Material & Supply Sales	P	P
Furniture, Furnishings, & Equipment Stores	P	P
Greenhouse Farming	P	T
Outdoor Sales & Displays of Merchandise	MUP	MUP
Plant Nurseries & Garden Supply Sales	P	P
Restaurants, Fast Casual	NP	P
Restaurants, Table Service	NP	P
Retail Stores, General Merchandise	P	P
<b>SERVICE USES</b>		
Auto/RV/Boat/Motorcycle Repair & Service	T	NP
Business Support Services	NP	P
Card Lock Fueling Facility	P	NP
Construction Yard	P	NP
Office Complex, Large Floorplate	NP	T
Offices, Accessory to Primary Use	P	P
Offices, Business & Professional	NP	P
Outdoor Storage & Work Areas	P	UP
Public Buildings & Structures	UP(2)	UP(2)
Public Safety & Utility Facilities	UP	UP
Repair & Maintenance - Consumer Products	P	P
Research and Development (R&D)	P	T
Snow Removal Business	P	NP
Veterinary Clinics, Animal Hospitals, Kennels, Boarding	P	MUP

TEMPORARY USES		
Commercial Filming	TUP	TUP
Off-site Contractor's Construction Yard	TUP	TUP
Offices, temporary	TUP	TUP
On-site Material Processing	TUP	TUP
Outdoor Retail Sales, temporary	TUP	TUP
Temporary Events, Non-profit Organization	TUP	TUP
Temporary Work Trailers	TUP	TUP

TRANSPORTATION, INFRASTRUCTURE & COMMUNICATIONS USES		
Commercial Parking and/or Vehicle Storage	UP	UP
Electrical Utility Facilities	UP	NP
Pipelines	UP	UP
Telecommunications Facilities	UP	UP
Tow Yard	UP	NP
Transit Stations & Terminals	UP	UP
Transit Stops Shelters	P	P
Utility Lines	P	P

### Key to Permit Requirements

SYMBOL	PERMIT REQUIREMENT
P	Permitted use. Zoning Clearance required for projects with less than 7,500 sq. ft. of floor area and less than 26,000 sq. ft. of site disturbance.
DP	Development Permit required for permitted projects with 7,500 sq. ft. or more of floor Area or 26,000 sq. ft. or more of site disturbance
NP	Not permitted.
UP	Conditional use. Use Permit approval required
MUP	Conditional use. Minor Use Permit approval required
T	Targeted Land Use (preferred use). Permitted. Development permit required ONLY for projects greater than 15,000 s.f. of floor area or 20 or more residential units; zoning clearance for less than 15,000 s.f.

**Notes:**

- (1) Definitions of the listed land uses are in Section 7 (Definitions, Glossary).
- (2) Permitted if the public agency use (e.g. office, outdoor storage, etc.) is permitted in zoning district; UP otherwise.

## General Development Standards

The Joerger Ranch Project shall be developed in accordance with the approved Overall Development Plan and consistent with the Town of Truckee CG Development Code standards, with the exception of those being modified by this Specific Plan, as shown on the applicable exhibits and as provided in the following table:

<b>(M) GENERAL DEVELOPMENT STANDARDS</b>		
	<b>M1</b>	<b>BIZ</b>
Front Setback	None	None
Side Setback	None	None
Rear Setback	None	None
Floor Area Ratio (FAR)	0.25 Maximum	0.25 Maximum
Site Coverage	No limitation other than minimum landscape requirements per Chapter 18.40 of the Development Code	
Open Space	No minimum	No minimum
Height Limit	50 feet	35 feet
Parking & Loading	As required by Development Code Chapter 18.48 (Parking & Loading Standards)	
Landscaping	As required by Development Code Chapter 18.40 (Landscape Standards)	
Outdoor Activities	All sales, displays and storage shall be conducted within an enclosed building, unless outdoor activities are approved in compliance with 18.58.190 (Outdoor Display and Sales Standards) and 18.58.200 (Outdoor Storage and Work Areas).	

3.1 | M1 - Manufacturing / Industrial



FIGURE 4-3.1: M1 ZONING DISTRICT

**Purpose**

The M1 Zoning District consists of approximately 9.6 acres along Soaring Way and is intended to allow for manufacturing and other industrial uses such as building material sales, auto/recreational repair and maintenance, warehousing space, specialty food production, etc. This zone is also intended to provide relocation sites for industrial and manufacturing uses from the Truckee River Corridor and areas of Downtown suitable for redevelopment.

**Target Uses**

- Manufacturing and warehousing
- Auto/recreational vehicle repair and maintenance

- Specialty food and spirit production facilities
- Relocated and new manufacturing, warehousing & processing
- Relocated and new specialty food and spirit production facilities
- Relocated and new lumber yard & home improvement center



Examples of Manufacturing / Warehousing Architecture

**Relocation Incentives for Targeted Uses**

One of the Plan’s objectives is to provide new zoning opportunities and incentives to encourage the relocation of some of the existing industrial/manufacturing businesses currently located along the Truckee Riverfront in the Downtown Specific Plan Area and also at key in-fill sites in the Downtown core. Particularly, there are sites within or adjacent to the Railyard Master Plan Area and along the Truckee River Corridor, that are underutilized and would be better suited for residential and/or mixed uses as opposed to the current auto repair and material supply uses. For example, although Truckee Tahoe Lumber is functional in its current location adjacent to the Railyard Master Plan Area, the long-term success of the Downtown, as envisioned by the General Plan, is tied to creating more residential and mixed-use opportunities. The same is true for the Truckee Riverfront. Although DR Autodoctor, Advanced Body and Frame, Top Dog and Timber Falling are functional in their current locations

along West River Street, these industrial uses would be better suited in the Joerger Ranch Specific Plan M1 zone.

Also many of the existing auto repair, welding, construction equipment yards and other industrial businesses along West and East River Street conflict with the Downtown River Revitalization Strategy and General Plan/Downtown Specific Plan goals and policies for riverfront beautification .

The M1 zone within the Joerger Ranch Specific Plan is an appropriate receiving zone to accommodate relocation of these existing industrial businesses. In order to incentivize relocation of industrial and manufacturing businesses located along the Truckee Riverfront and within the Downtown Specific Plan Area the following special site development standards and guidelines may be applied if the vacated property is backfilled with new land uses supported by the Downtown River Revitalization Strategy .

- Increased site coverage
- Decreased architectural standards for warehouse building types
- Outside storage and work areas as a permitted use
- Options for offsite parking and snow storage
- Decreased landscape/open space requirements
- Waiver of the Workforce Housing requirements
- Zoning Clearance land use permit review for any size project
- Waiver of land use application filing fees

#### **Relocation Development Standards**

The following standards shall apply to businesses that can verify to the Community Development Director's satisfaction that they are relocating all business operations from a Downtown Specific Plan infill area or River Protection Overlay District site. The

business owner must provide proof that the business was in operation prior to the adoption of this Specific Plan. New businesses that cannot verify relocation shall comply with the General Standards and General Design Guidelines listed in Section 4.3.

**RELOCATION-1:** A minimum of 10% of the site shall be landscaped with a combination of trees, shrubs, and ground cover including a minimum of a five-foot wide strip along street frontages and 300 sq. ft. at building entrances with public access. This requirement supersedes compliance with Development Code Chapter 18.40.

**RELOCATION-2:** All relocated uses are considered exempt from Workforce Housing requirements.

**RELOCATION-3:** Off-site parking, including parking with the Town right-of-way, may be permitted through Minor Use Permit approval.

**RELOCATION-4:** Truckee Tahoe Lumber shall be exempt from Chapter 7 infrastructure requirements if a building permit is issued within three years of the Phase I Parcel Map recordation date (per Council Resolution No. 2015-11).

#### **Relocation Design Standards**

The following design guidelines are specific to relocated uses as described above and shall not apply to new businesses within the M1 zone.

**RELOCATION G1:** Buildings shall be designed to reflect the intended use (i.e., an auto repair shop should include roll-up doors, areas for car storage and repair). The review authority may consider the use of prefabricated metal buildings and more simplistic building designs providing all points of public access are well-defined and the building is constructed of durable materials.

**RELOCATION G2:** The use and business functionality shall be the primary consideration for building and parking locations.

3.2 | BIZ - Business Innovation Zone



FIGURE 4-3.2: BIZ ZONING DISTRICT

**Purpose**

The BIZ zoning district provides approximately 11.6 acres of land along Martis Drive, intended to attract new innovative businesses focused on high tech, R&D, culinary and green technology. The BIZ zoning district is also intended to diversify Truckee’s employment base by attracting businesses with year-round, high quality, full-time jobs. The key to this area is a campus style business environment in support of the business innovation cluster. Eco-friendly and emerging green industries are encouraged.

The BIZ-1 area located within the northwest portion of the site between Martis Drive and the Ponderosa Golf Course identifies site specific development standards intended to reduce land use conflicts and provide a greater separation between the off-site residential uses and future development within the BIZ-1 zoning area. The northern portion of the BIZ-1 area is heavily treed and steeper slopes. In addition to the BIZ Development Standards and

Design Guidelines, development within the BIZ-1 area shall be limited to the designated building envelope shown on Figure 4.3.2A and subject to the following Design Controls Standards:

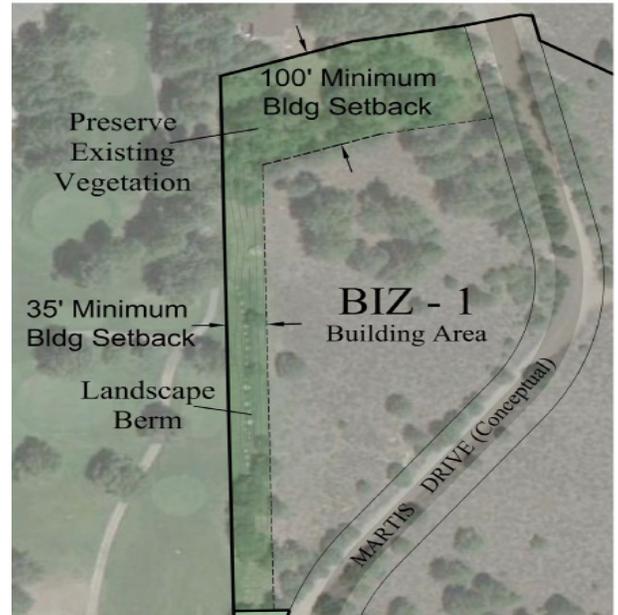


FIGURE 4-3.2A: BIZ-1 BUILDING AREA

- Building height shall not exceed 35 feet
- Load docks, loading bays or vehicular access doors are prohibited on north and west side of any building.
- Vehicular parking and truck access is prohibited on north and west side of any building.
- Nighttime activity shall be restricted to within the building.
- Building entrance, parking, loading and activity centers shall be ordinated to the east toward Martis Drive.
- An earthen and landscaped berm shall be constructed concurrent with Martis Drive Roadway improvements along the west property line as shown on Figure 4.3.2

### **Target Uses**

- Specialty food and spirit production & distribution
- Research and Development Facilities including high tech industries (software, social media, commercial hardware, electronics, etc.)
- Green technology including material production, design & research
- Culinary uses

**BIZ-G3:** Existing trees in the northern area of the BIZ zoning designation shall be preserved where feasible to buffer the BIZ use from the existing residential neighborhood to the north.

### **BIZ Development Standards**

In addition to the M Development Standards, the following standards shall apply to the BIZ district:

**BIZ-1:** Parking areas shall be screened from Martis Drive by landscaping at a ratio of 2 trees and 4 shrubs for every 5 parking spaces.

**BIZ-2:** Outdoor lighting shall be limited to the minimum necessary and shall be shielded downward to avoid light spill.

**BIZ-3:** An earthen and landscaped berm shall be constructed along the western property line of the BIZ-1 zone concurrent with Martis Drive roadway improvements.

### **BIZ Design Standards**

In addition to the M Design Guidelines, the following guidelines should be considered for development within the BIZ district:

**BIZ-G1:** Create a “campus style” Business Center atmosphere that promotes innovation and flexibility through the use of simple building forms.

**BIZ-G2:** Separate “campus” buildings shall be connected with walkways and plazas for the purpose of creating an environment conducive for exchange of business ideas and shared amenities.

## 4 | Workforce Multi-Family Residential (RMW-20) Zoning District



FIGURE 4-4: RMW-20 ZONING DISTRICT

### **Purpose**

The Workforce Multi-Family Residential (RMW-20) Zoning District is approximately 4.0 acres located along Martis Drive and is intended to put workforce housing units near employee-generating commercial and industrial uses. The RMW-20 Zoning District is the primary workforce housing location for the Joerger Ranch Specific Plan. Development within this zone is limited to high density residential units between 18 and 20 dwelling units per acre and will provide most of the projected workforce housing for buildout of the Specific Plan Area. The standards and guidelines established by this section supersede Development Code regulations for multi-family projects (Development Code Section 18.58.180,

Multi-Family Residential Projects) and workforce housing requirements (Development Code Chapter 18.216, Workforce Housing). The RMW-20 zone is considered a unique zone, specific to the needs of the Joerger Ranch Specific Plan and as such, it is the only area of multi-family development within the Specific Plan Area that has its own standards. Multi-family units constructed within the Specific Plan Area outside of the RMW-20 zone shall be subject to Development Code requirements, including that all residential projects are required to provide Inclusionary Housing in compliance with Development Code Chapter 18.214.



FIGURE 4.4A - AIRPORT OVERFLIGHT ZONES

The location of the RMW-20 Zoning District was selected to minimize noise and compatibility conflicts with adjacent roadways, future anticipated land uses and the Truckee-Tahoe Airport. The RMW-20 zone is located in Zone D of the Truckee-Tahoe Airport

Land Use Compatibility Plan, an area acceptable for residential development, and adjacent to existing residential, hotel and commercial uses where a higher density multi-family project would be most appropriate.

The photos included in this section identify general ideas of mass, scale, density and architecture and should not be viewed as design solutions for specific building applications, but rather concepts of development of the site.



Example of Integrated Parking & Varied Roof Design

### **RMW-20 Development Standards**

Development Standards for development within the RMW-20 Zoning District of PC-3 are described below:

**RMW-1:** Dwelling unit sizes shall range in size from 400 square feet of gross floor area to a maximum of 1,000 square feet of gross floor area. Garages, common areas including shared stair wells and entries, externally accessed storage areas and similar features shall be excluded from the unit size calculation.

**RMW-2:** A maximum of 20% of the total workforce housing units may be 1,000 sq. ft. with no limitations on the number of bedrooms. The remaining units shall be a mix of studio, one-, and two-bedroom units (JR Policy P8.4).

**RMW-3:** A minimum of 25 percent of the dwelling units shall have 40 square feet or more of private exterior deck or patio that is internally accessible from the unit.

**RMW-4:** Common open space shall be provided as follows:

- Development shall be designed to provide the equivalent of a minimum 100 square feet of common open space for each dwelling unit.
- The open space shall be located to maximize solar exposure and shall include areas for passive recreation and community interaction such as but not limited to a bocce court, horseshoe pit, picnic area, grass sitting area, gas fire pit with seating, etc.

**RMW-5:** Parking shall be calculated as follows:

- Minimum 1 space per unit, surface or garage parking and maximum 2 spaces per unit.
- Guest parking at a minimum of 15% and maximum 20% of the total required spaces.
- Minimum two electric vehicle charging stations. May be combined with guest parking.



Example of Varied Street Frontage Design

**RMW-6:** To ensure construction of a comprehensive development, raw land subdivisions where concurrent land use approval for project construction has not been issued, shall be prohibited.

**RMW-7:** The installation of woodstoves and other wood burning devices within individual units shall be prohibited.

**RMW-8:** Development within the RMW-20 zone shall comply with the policies under JR Goal 8.

**RMW-9:** Workforce housing units shall be restricted to full-time occupancy. Rental units as short-term (less than 30 consecutive days) rentals shall be prohibited.

**RMW-20 Design Standards**

**RMW-G1:** Buildings shall be oriented to create courtyards and common open space area to facilitate community interaction.



Example of Defined Entries & Private Common Area

**RMW-G2:** Housing and outdoor activity shall be sited with consideration given to golf “safety zones” along the Ponderosa Golf course.

**RMW-G3:** The minimum number of bicycle parking spaces shall equal 25 percent of the parking demand for the project. At least 45 percent of the required bicycle parking spaces shall be provided as enclosed bicycle parking spaces, such as bicycle lockers. At minimum, the number of bicycle

parking spaces provided within 50 feet of each mixed-use building or residential building shall be equivalent to the number of residential units included in each building.

**Architectural Design**

**RMW-G4:** Although there is no particular “style” proposed for multi-family residences in the Joerger Ranch Specific Plan, the intent is to create a high-quality residential project for workforce housing. As such, projects within the RMW zoning district shall include the following architectural design elements:

- Richness of surface and texture through the use of high quality materials & architecture
- Wall articulation (e.g., insets, popouts, cantilevers, etc.)
- Roof overhangs with regulated window rhythm
- Defined entries



Example of Modern Architecture

**RMW-G5:** Trail and walkway systems shall connect the multi-family housing units to the overall trail system and commercial/retail areas within PC-3.

**RMW-G6:** Units shall be constructed to maximize privacy between units.

**RMW-G7:** Use overhangs, secondary forms (e.g., porches, balconies, trellises, patios and low walls), purposeful window variation and similar features to provide additional visual interest.

**RMW-G8:** Architectural design shall incorporate energy efficiency, such as natural heating and/or cooling, sun and wind exposure and solar energy opportunities.

**RMW-G9:** Building entrances shall be distinctive and easily identifiable.

#### FORMS AND MASSING

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**RMW-G10:** Building masses shall be broken up through varied roof planes, height, etc. Buildings shall include a mixture of 2- and 3-stories where density can be achieved.

**RMW-G11:** The overall form and massing for the multi-family housing is based on combining one or more central forms of simple geometry with added secondary elements to create visual interest without being overly complex.

**RMW-G12:** Long, unbroken building façades and simple box forms are not permitted. Building façades shall be broken up to give the appearance of a collection of smaller buildings through the use of balconies, setbacks and projections to help articulate individual units (or collection of units) and through the pattern and rhythm of windows and doors.

#### SCALE

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**RMW-G13:** Reduce building scale through window patterns, structural bays, roof overhangs, porches, awnings, moldings, fixtures, details and landscaping.

**RMW-G14:** Minimize building scale by incorporating separations, changes in wall plane and height, and the inclusion of elements including balconies, porches, arcades, dormers and cross gables, secondary hipped or gabled roofs, and variety in roof pitch and design to mitigate the

“barracks-like” quality of flat walls and roofs of excessive length.

#### ENTRY IDENTITY

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**RMW-G15:** The use of shared common entries shall minimize the potential repetitive effect of side-by-side entry doors for individual units.

#### GARAGES AND PARKING

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**RMW-G16:** Placement of multiple side-by-side garage doors fronting directly onto the adjacent streets is not permitted.

**RMW-G17:** Garages shall be offset to allow for diversity. Porches, entryways and other secondary components shall be used effectively to lessen the visual impact of the garage doors from the street.

**RMW-G18:** Recessed garages shall be used to de-emphasize the massing of garages.

**RMW-G19:** Special paving at parking court entries and landscape nodes between parking stalls shall be utilized to soften the streetscape.

#### BUILDING MATERIALS

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**RMW-G20:** The following materials are permitted for buildings:

- a. **STONE** Stone such as weathered granite, basaltic rock, or natural stone to express structural mass in walls and chimneys. Stone walls shall strive to appear load-bearing, as opposed to a thin veneer.
- b. **WOOD BOARD AND BATTEN** Vertical board and batten, as well as reversed wide battens over boards or rough sawn plywood with batten, in various sizes and widths.
- c. **CONCRETE** Board-form concrete.
- d. **STUCCO** Dark colored plaster or smooth finished stucco.
- e. **METAL & COMPOSITE SIDING** Metal siding and durable composite siding as an alternative

to wood siding. When used, metal siding shall be allowed to naturally weather.

- f. **WOOD SIDING** Various sizes and profiles of wood siding may be used in horizontal or vertical patterns. Stain, semi-transparent stain and painted wood are permitted.



Appropriate Use of Mixed Materials and Varied Unit Types

#### WINDOWS AND DOORS

**RMW-G21:** Exterior doors at the front entries shall be designed to create an individual identity for each unit, while being compatible and harmonious with the architectural character.

#### ROOFS

**RMW-G22:** Roofs shall convey and establish scale and interest through use of varied pitches and forms.

**RMW-G23:** Roof overhangs shall be used to shade large glass areas and avoid reflective glare.

**RMW-G24:** Roof forms shall be designed for snow shedding. The roof plan shall be designed in unison with the site and landscape plans to avoid conflicts with drainage and safety.

**RMW-G25:** The technical design of roofs, such as detailing for ventilation, insulation, gutters and similar uses, shall be designed to blend into the background.

**RMW-G26:** Attached housing units shall provide varying roof ridgelines and avoid repetitive architecture.

**RMW-G27:** The design of roof types for multi-family structures shall utilize the following roof types and techniques:

- a. Gable, shed, hip and flat roofs
- b. Combining roof types
- c. Creating articulation in ridgeline plane
- d. Varying plate heights and ridge heights
- e. Changing roof levels and ground planes to break up the mass and bulk of buildings

#### GUTTERS AND DOWNSPOUTS

**RMW-G28:** The overall design and strategic placement of roof forms are the primary means of effectively managing snow melt / snowshed.

**RMW-G29:** Gutters, downspouts and flashing shall be fabricated from steel and colored to blend with the overall color scheme of the individual units.

**RMW-G30:** Flashing, gutters and downspouts shall be minimized in their appearance.

#### COLORS AND TRIM

**RMW-G31:** Siding and timbers shall be treated or stained in semi-transparent finishes to enhance the natural colors and qualities of the wood.

**RMW-G32:** Use earth tones to be compatible with the natural setting. Exterior colors shall appear natural and blend into the surrounding landscape.

**RMW-G33:** Details and trim shall be used to establish individual identity and interest in each unit. Wood and vinyl are permitted materials for trim and window frames.

**RMW-G34:** Colors for pre-finished window frames, mullions and divided lights shall be complementary colors found naturally on-site, including pine and

sage green, reddish-browns, tans and similar colors.

#### CHIMNEYS, FLUES AND ROOF VENTS

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**RMW-G35:** Chimneys may be incorporated into the design of the multi-family housing units.

**RMW-G36:** Large flues and vents shall be consolidated and enclosed within a chimney-type enclosure. Small, exposed flues, such as plumbing vents, shall be painted to match the adjacent roof.

**RMW-G37:** All roof projections including, but not limited to, flues and vents, shall be compatible in height and material with the structure from which they project. All projections and safety features shall comply with the California Uniform Building Code (UBC) and Town of Truckee requirements.

#### TRASH ENCLOSURES

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**RMW-G38:** All trash, including recycling, shall be stored in an enclosure integrated into the site and have accessibility for sanitation pickup.

#### EXTERIOR EQUIPMENT

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**RMW-G39:** All exterior mechanical, electrical and other utility equipment, such as metering devices, transformers, gas meters and satellite dishes, shall be substantially screened from public view and adjacent structures. Wall-mounted utilities shall be enclosed.

#### FENCES & WALLS

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**RMW-G40:** In order to maintain the visual quality of an open and natural landscape, fences and walls within the multi-family housing areas shall be minimized, except to enclose private spaces for individual units. Fences and/or walls designed to enclose private spaces shall not exceed 6 feet in height.

**RMW-G41:** Fences and/or walls shall not be used to define or enclose property boundaries.

## Permitted Land Uses

**Table 4.4A** identifies the uses allowed by this Specific Plan in the workforce multi-family residential district, and the land use permit required to establish each use.

LAND USES (1)	RM
Cohousing	P
Workforce multi-family housing, for rent or sale	T
Residential Accessory Use or Structure (Refer to Section 17.44.020 of the Development Code)	P

## Key to Permit Requirements

SYMBOL	PERMIT REQUIREMENT
P	Permitted use. Zoning Clearance required for projects with less than 10 units.
T	Targeted Land Use (preferred). Permitted. Development permit required ONLY for projects greater than 10 units.

**Notes:**

(1) Definitions of the listed land uses are in Section 7 (Definitions, Glossary).

## General Development Standards

The Joerger Ranch Specific Plan shall be developed in accordance with the approved Overall Development Plan and consistent with the Town of Truckee RM Development Code standards, with the exception of those being modified by this Specific Plan, as shown on the applicable exhibits and as provided in the following table:

RMW-20 GENERAL DEVELOPMENT STANDARDS	REQUIREMENTS
<b>Lot Size</b>	
Minimum Area	None
Minimum Width	None
<b>Setbacks</b>	
Front	10 ft.
Side Interior	5 ft.
Street Side	5 ft.
Rear	10 ft.
Site Coverage	N/A
Height Limit	35 feet

Parking

- **Minimum** of 1 parking space per unit / guest parking 15% of total required spaces
- **Maximum** of 2 parking spaces per unit (garage or surface)
- Minimum 2 electric vehicle charging stations

## 5 | Signage

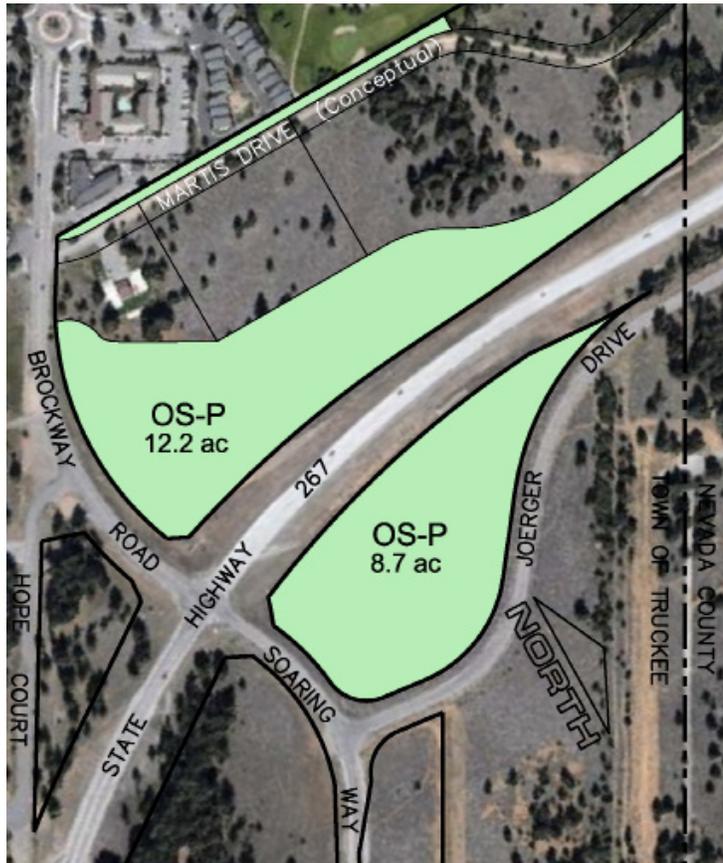
(Refer to Town of Truckee Development Code, 18.54)

## 6 | Landscaping

(Refer to Town of Truckee Development Code, 18.24.040.B)

## 7 | Lighting

(Refer to Town of Truckee Development Code, 18.24.040.B)



## 8 | Open Space

More than 20 acres of permanent Open Space are provided along the Highway 267 corridor and a 30' wide open space strip is proposed along the north side of Martis Drive. The OS-P zoning district is primarily intended to protect natural resources and provide permanent, undeveloped open space in perpetuity and establish a visual open space corridor along State Route 267.

Only limited uses such as public trails, drainage, road & utility infrastructure, snow storage, and similar land uses are permitted within this zone. Existing trees within the 30' open space strip along Martis Drive shall be preserved where feasible. If Martis Drive is relocated within any portion of the OS-P zoning district, an equivalent amount of open space shall be replaced as additional buffer

along the western boundary of Parcel 4, north of Brockway Road.  
 For open space outside of the OS-P Zoning District, refer to Development Code, Chapter 18.46.

### Permitted Land Uses

Table 4.8A identifies the uses allowed by this Specific Plan in the Open Space district.

LAND USES	OS-P
Drainage Facilities	P
Public Trails	P
Road/Utility Infrastructure	P
Snow Storage	P

### Key to Permit Requirements

SYMBOL	PERMIT REQUIREMENT
P	Permitted use.

## 9 | Green Building Design Concepts

### Green Design

“Green Design” is also commonly known as “Sustainable Design”. The objective of Green Design is to eliminate or reduce negative environmental impact through skillful, sensitive design, including use of resources that are renewable, thereby reducing environmental impacts and ensuring that resources are available for future generations.

Green Design concepts are encouraged to be applied to all site plan, building, drainage and landscape designs used within each zoning district in the Joerger Ranch Specific Plan. Incorporating “green” building concepts into project designs provides the following benefits:

1. Conserves natural resources
2. Reduces automobile dependence
3. Reduces utility costs
4. Improves air and water quality
5. Contributes to overall “quality of life”
6. Reduces solid waste
7. Enhances comfort and health
8. Optimizes lifecycle economic performance
9. Minimizes strain on local infrastructure

### Green Design Principles

1. **BICYCLING** – Promote bicycling and transportation efficiency by linking development to the Truckee trail system. Non-residential and multi-family residential buildings shall provide secure bicycle parking spaces or storage for at least 5% of the parking space capacity provided for automobiles.



2. **OPEN COMMUNITY** – Design all streets and sidewalks in the Plan area to be available and easily accessible for general, public use and not gated.
3. **WALKABLE STREETS** – Provide appealing, safe and comfortable streets and outdoor spaces in order to promote pedestrian activity. All projects should incorporate the following features to the extent feasible:
  - a. A principal, functional entry on each building that has a front façade facing the street, plaza or public space.
  - b. Continuous sidewalks or equivalent provisions for walking along both sides of all streets within the project.
4. **ENERGY EFFICIENCY IN BUILDINGS** – In January, 2010, California adopted new energy efficiency standards for all building permits. All buildings within Joerger Ranch are required to meet or exceed those standards which require all new construction to reduce energy use through the use of energy efficient equipment and appliances. Some energy saving concepts for consideration are discussed under “Power Consumption”.
5. **REDUCED WATER USE** – Minimizing water use in buildings is encouraged through the use of “low-flow” appliances and toilets.

Landscaping that requires less water and/or does not require permanent irrigation systems is also encouraged, thereby further reducing the impact to natural water resources and the burden on local serving utilities for water supply and wastewater systems.

6. **SOLAR ORIENTATION** – Achieve enhanced energy efficiency by creating optimum conditions for the use of passive and active solar strategies.

When possible, the longer axis of the building should be oriented east/west so that the longer dimension of the building is facing within 15 degrees of true south.

Proper placement of windows can cut the building's total energy use by 30 - 40 percent at no extra cost. Spaces such as garages, storage or laundry rooms can be situated on the building's east/west facing "short side" where they act as an additional thermal (or heat) buffer.

7. **LIGHT POLLUTION REDUCTION** – Minimize light trespass from the site, reduce sky-glow to increase night sky access, improve nighttime visibility through glare reduction and reduce development impacts on nocturnal environments.
8. **REUSE & RECYCLING** – Where possible, recycle and/or salvage for reuse non-hazardous construction debris. Use recycled content, where possible, for construction materials such as paving, road base and plastic pipe materials.
9. **MINIMIZE SITE DISTURBANCE** – Conserve existing natural areas, existing vegetation and trees to protect habitat and promote biodiversity. Reduce impervious surface area and associated impacts by use of pervious pavement, pavers and low impact development (LID) stormwater facilities, where feasible.

## Power Consumption

Incorporating the following energy-saving concepts into building designs will reduce energy consumption and increase personal comfort. These concepts are not mandatory but are encouraged:

1. **NATURAL COOLING** – Any combination of natural cooling techniques should be used to reduce overheating buildings. Deep window overhangs and/or trellises, primarily on south and west facing glass, provides a balance between summer cooling and winter heating through solar gain. Where feasible, deciduous trees should be located to shade west facing windows. Natural cooling reduces the need for air conditioning, saves money on energy bills and utilizes "fresh air" to make buildings more comfortable.
2. **PASSIVE SOLAR HEATING** – Passive solar systems provide heat to the building structure through south facing windows in conjunction with thermal mass. Locate windows within 30° of due south and store excess heat in massive elements, such as slab floors or stone fireplaces. Passive solar design can reduce heating requirements by 30% to 40%.
3. **SOLAR WATER SYSTEMS OR PRE-PLUMBING FOR FUTURE SOLAR WATER HEATING** – Consider installing solar water heating systems OR pre-installing insulated copper pipes from the attic to a hot water closet or mechanical room for future solar installation. Sufficient south facing roof area for collectors and access to mechanical room piping should be considered.
4. **PHOTOVOLTAIC (PV) SYSTEMS** – PV systems collect the sun's energy and convert it into electricity. Excess electricity may be sent back into the utility grid for a credit on electric bills. All PV systems shall be integrally designed into the roof structure.

5. **RADIANT HEATING SYSTEMS** – Radiant heating is up to 30% more efficient than forced air heating systems. Radiant heat may be installed in zones that allow users to adjust the temperature in various areas of the building, based on usage and desired comfort level.
6. Construction quality to meet or exceed typical Energy Star®, HERS, air leakage and duct leakage testing.
7. High efficiency lighting and controls.
8. The installation of Energy Star® certified appliances is strongly encouraged. These appliances are significantly more efficient in their use of water and electricity. Most Energy Star® appliances are available in stainless steel, black, white and cabinet integrated finishes.

### Wood Materials

1. Reclaimed wood helps reduce the number of trees harvested for building construction. With its usual attractive and rustic appearance, reclaimed lumber gives buildings a timeless character.
2. If new wood is used, consider lumber certified by the Forest Stewardship Council (FSC). The FSC provides a credible guarantee that the lumber comes from a well managed forest. FSC certified lumber generally does not cost any more than non-certified lumber and assures you are not contributing to clear cutting practices that degrade ecosystems.

### Window Selection

1. Consider utilizing Energy Star® labeled windows. These windows are twice as efficient as the average window produced just ten years ago. The products are designed to reduce heat loss and increase solar gain to provide warmer buildings

in the winter and cooler buildings in the summer. Energy Star® windows are available in every aesthetic design and are operable to allow fresh air ventilation during summer months. Product features include dual panes, low-emissive coatings (Low-E) and wood or compost frames.

### Insulation

1. Consider upgrading insulation to exceed California Title 24 requirements. Properly installed, insulation will reduce the demand for heating and cooling, making buildings more comfortable and energy efficient.
2. Preference should be given to loose and spray cellulose insulation products which are made from 100% recycled newspaper and treated with borates for fire and insect resistance. Spray cellulose wall insulation is mixed with less toxic binders to adhere to stud and joist cavity surfaces, while completely filling cavities and reducing air movement within wall cavities, deterring moisture intrusion and flame spread. It also reduces infiltration, further contributing to a quieter, more comfortable and energy efficient building. Cellulose insulation is also formaldehyde-free which preserves air quality.
3. Cotton batt products should be used for batt insulation methods. These products tend to use recycled cotton products, including denim, do not itch, and do not contain chemicals that degrade air quality. When using batt insulation, expandable foam and caulk shall be used to prevent infiltration. Holes between floors and between stud cavities around wire runs shall be sealed. Caulk top and bottom plates on all floors.
4. Fiberglass should be used only as a last resort. Many fiberglass insulation products include recycled glass, formaldehyde-free binders, non-asphalt adhesives or colored dyes. Formaldehyde-

free binders reduce indoor air quality problems. Fiberglass insulation may contain up to 30% recycled glass.

### Paints and Stains

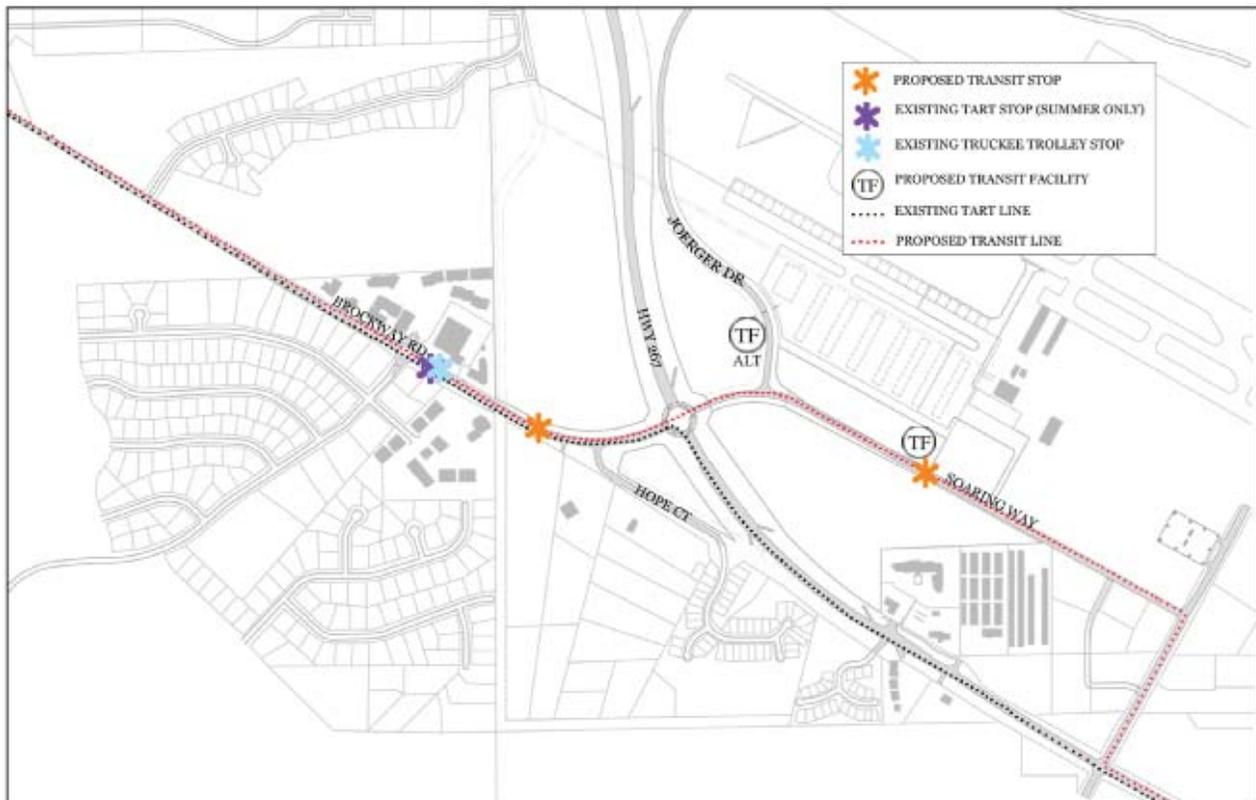
1. Consider the use of Green Seal<sup>®</sup> certified products. Green Seal<sup>®</sup> is an independent, non-profit organization that strives to achieve a healthier, cleaner environment by identifying and promoting products and services that create less toxic pollution and waste.
2. When Green Seal<sup>®</sup> products are not practical, consider low Volatile Organic Compound (VOC) products. These products are better for air quality in their use and manufacturing. In interior applications, low VOC products significantly reduce the emission of chemicals. Many of these products are water based to facilitate easier clean-up.

### Paving and Hardscape

Consider using permeable surfaces that allow water to percolate into the soil. For driveways, walkways and patios, consider gap-spaced unit pavers, decomposed granite, gravel or grass-stabilization systems. Allowing storm water percolation reduces the volume of storm water runoff and sheet flow, while replenishing local aquifers and reducing soil erosion. Additional benefits include reducing irrigation requirements and lowering the risk of flooding. These permeable surfaces may be effectively used in combination with biofiltration, bioswales and subsurface detention systems.

# Chapter 5

## Transportation



# 1 | Introduction

This chapter describes the vehicle, pedestrian and transit systems within the Joerger Ranch Plan Area and the respective relationships planned to interface with the existing and future road, trails and bikeways network .

Unlike many new development areas, there is already a significant existing public road and highway network that serves Joerger Ranch and the greater Truckee Area. The existing road and highway network (see Figure 5-1) provides easy access to and from the plan area. The site is bisected by State Route Highway 267, creating separate development areas

with varying levels of highway visibility. Similarly, but to a lesser degree, Soaring Way and Brockway Road further separate the development into four (4) quadrants within the Planning Area. SR 267 is an existing State Highway and direct access from the highway to the development is restricted. Brockway Road and Soaring Way are existing minor arterial roadways, and provide the primary access to the Plan area.

The Joerger Ranch Specific Plan is designed to provide vehicular, pedestrian and bicycle circulation within the Planning Area and is consistent with the Goals, Policies and Guidelines of the General Plan and the Town’s Trails and Bikeways Master Plan.

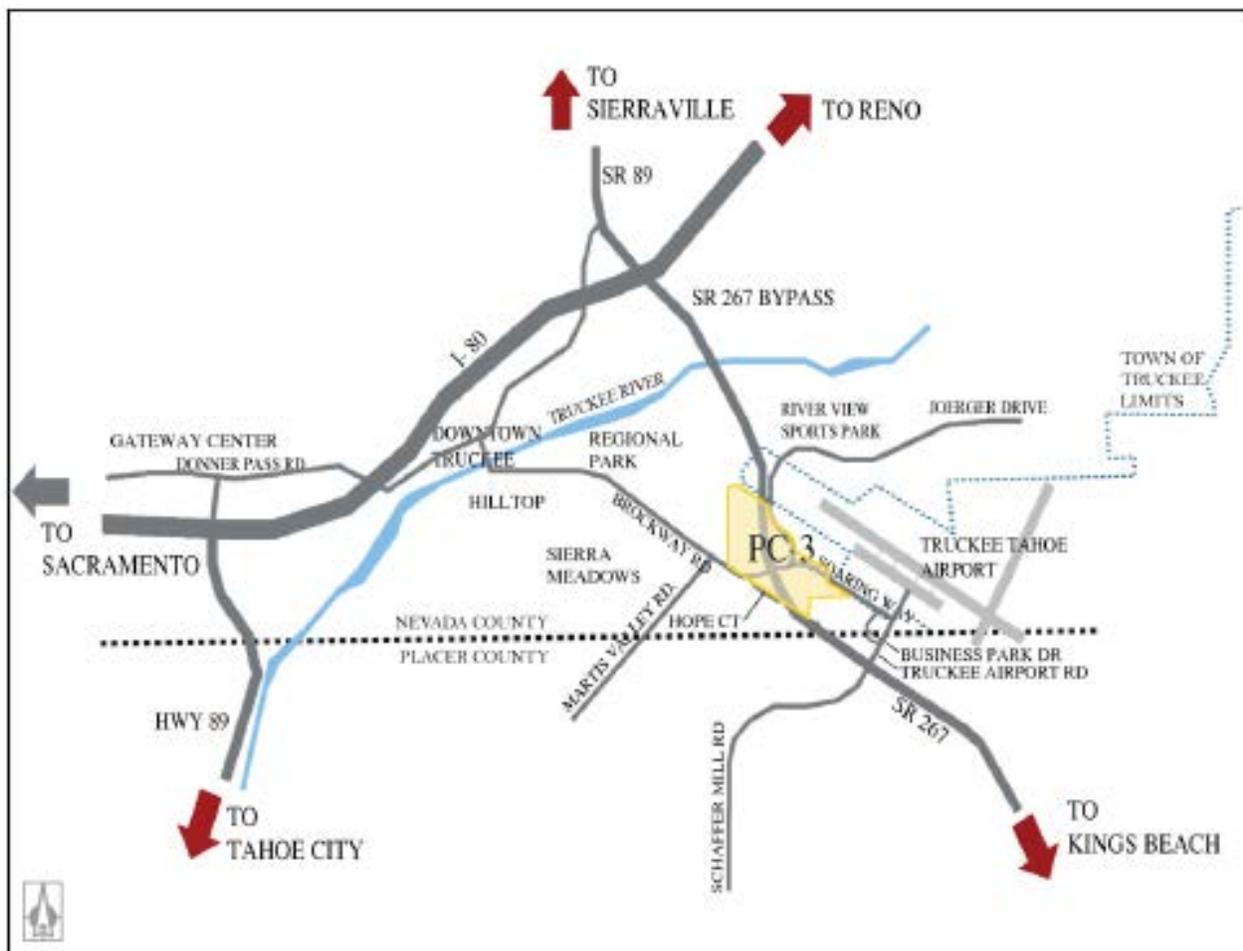


FIGURE 5-1 ROADWAY NETWORK

Key circulation goals are as follows:

**Goal CIR-1:** *Develop a roadway system that provides access and mobility for Truckee residents and businesses and adequately serves existing and planned land uses while maintaining local community character.*

**Goal CIR-2:** *Maintain adequate Level of Service on Truckee’s roadways and intersections to ensure the safe and efficient movement of people and goods throughout the Town.*

**Goal CIR-4:** *Create new developments that are integrated into the circulation network and promote connectivity within and between community areas.*

**Goal CIR-10:** *Provide a safe, comprehensive, and integrated System of facilities for pedestrians and cyclists and other non-motorized modes of transportation.*

Additionally the key element specific to Joerger Ranch identified in the Town’s General Plan Circulation element states as follows:

*Per General Plan Policy PC3-P8, the mix of land uses in the Joerger Ranch Specific Plan will not generate an amount of traffic that, in addition to the buildout of the General Plan, will result in the need for four lanes on Highway 267 from Highway 80 to the Highway 267/Brockway Road/Soaring Way intersection.*

The Joerger Ranch Specific Plan improves existing roadway circulation patterns by improving existing roadways & intersections with added lanes, striping, crosswalks and new bicycle and pedestrian links. These improvements help support the different land uses within Joerger Ranch and integrate the needs of motorized vehicles, bicycles and pedestrians alike for safe conveyance of travel. Detached bike paths,

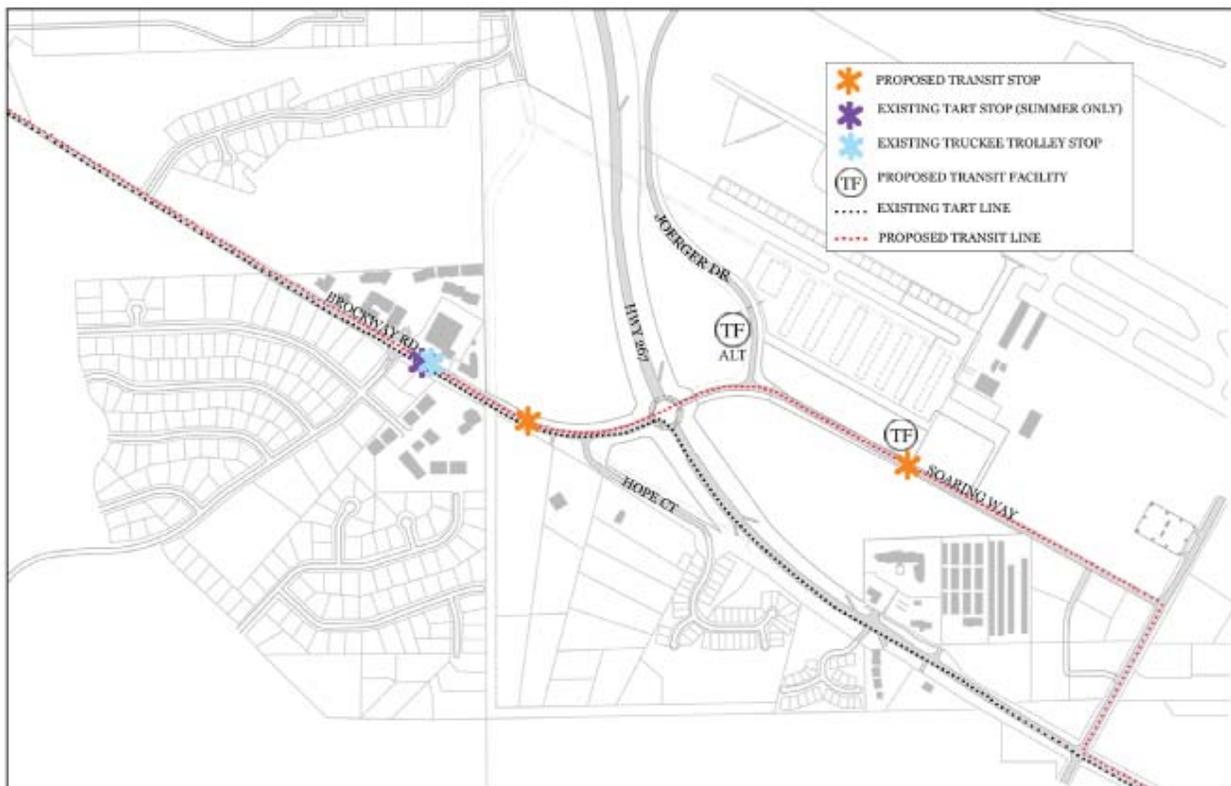


FIGURE 5-2 PUBLIC TRANSPORTATION

re-striping and roadway widening are some of the identified improvements.

Intersections are a key element to providing effective circulation. The Joerger Ranch Specific Plan proposes various improvements to the following four (4) intersections which are described in further detail in Section 2:

- Brockway Road / Martis Drive
- Brockway Road / Hope Court
- Highway 267/ Brockway Road / Soaring Way
- Soaring Way / Joerger Drive

General Plan Table CIR-5, item 25 identifies the Highway 267/Brockway Road/Soaring Way intersection to be improved with a “roundabout, or additional through and turning lanes”. As illustrated in Figure 5-14, the Joerger Ranch Specific Plan identifies additional through and turning lanes along with improvements for the safe crossing of pedestrians and bicyclists. The identified improvements may be constructed incrementally or all at once, as traffic demands warrant to meet the requirements of General Plan Policy 2.1. This intersection is also identified within the Town’s AB 1600 Traffic and Facility Impact Fee Program and traffic fees are currently being collected for required future improvements.

Bike paths are an important component of the Joerger Ranch Specific Plan. Separated 10’ wide Class 1 bike paths and attached Class II bike lanes are proposed to create bicycle and pedestrian connectivity within the Plan Area and greater Truckee Area by providing linkage to future connections to the Truckee River Regional Park, Truckee River Legacy Trail, River View Sports Park and Martis Valley Regional Trail connecting Truckee to Northstar. The design of Class I paths and Class II bike lanes will follow the specifications of the Town’s Trails and Bikeways Master Plan.

Sidewalks, pedestrian paths, and bike trails are proposed both on-site and off-site within and around the Plan Area. Crosswalks will be utilized to promote pedestrian safety and encourage non-motorized transportation and are integrated throughout the Plan Area. This is critical in order to create a walkable, pedestrian accessible environment that is currently dominated by vehicular traffic. Utilizing materials with textures and colors at crossings to distinguish pedestrian crossings from vehicular traffic is an important design characteristic. The primary Class 1 bike path crossing near the Brockway Road/Hope Court intersection shall consider additional design elements to provide safety for bicyclists and pedestrians including raised medians with a “crossing island” to shorten the crossing distance and shall be approved by the Town Engineer. An alternative location may be approved by the Town Engineer based on the outcome of a future corridor study for this section of Brockway Road.

The Circulation Element of the General Plan supports alternate modes of transportation to reduce vehicular use. The Joerger Ranch Specific Plan enhances bicycle and pedestrian circulation with the construction of sidewalks and pathways along with the incorporation of bus shelters on Brockway Road and Soaring Way (*Figure 5-2*).

Sections 2 and 3 of this chapter identify specific roadway and circulation improvements proposed for the Joerger Ranch Specific Plan Area.

## 2 | Roadway & Intersection Design

### 2.1 Roadway Improvements

The Plan Area requires different roadway sections to meet the circulation needs based on existing traffic patterns and proposed land uses contained within the Specific Plan. Proposed roadway sections combine and integrate vehicular, bicycle and pedestrian uses for safe conveyance of travel.

**SOARING WAY:** Soaring Way in its existing condition consists of 12' travel lanes, 8' paved shoulders, and concrete curb/gutter.

The proposed roadway section for Soaring Way, east of Joerger Drive, is illustrated in Figure 5-3. Sidewalks (6' wide) will be added to each side of the existing road section. The roadway will be restriped to include 11' wide travel lanes, 5' Class II bike lanes and on-street parking on the southwest side of the street.

Prior to development of Parcel 2, on-street parking on the northeast side of Soaring Way will be evaluated. The final configuration is at the discretion

of the Town Engineer. Future widening of Soaring Way to accommodate additional on-street parking, if needed, would be the responsibility of the Parcel 2 development.

The proposed roadway section of Soaring Way, west of the Joerger Drive roundabout, is a transitional section to Highway 267 and is illustrated in Figure 5-4. It will include sufficient width to accommodate a right turn lane, left turn lane, and through lanes. Additionally, the roadway section will include two 5' wide Class II bike lanes and a Class I bike path on both sides. A different configuration may be required if a roundabout at SR 267 is constructed.

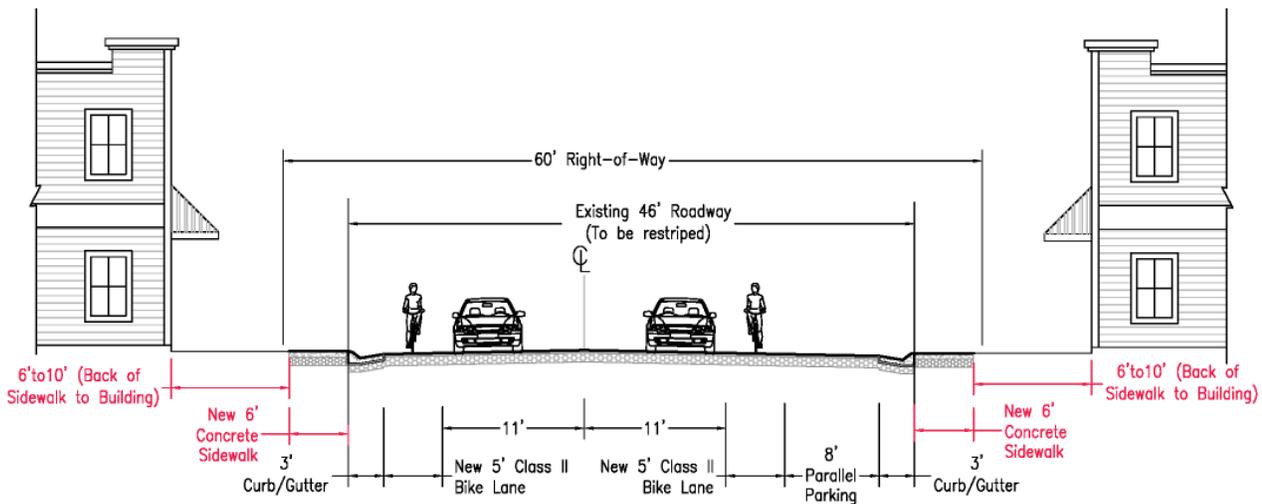


FIGURE 5-3: SOARING WAY - EAST OF JOERGER DRIVE

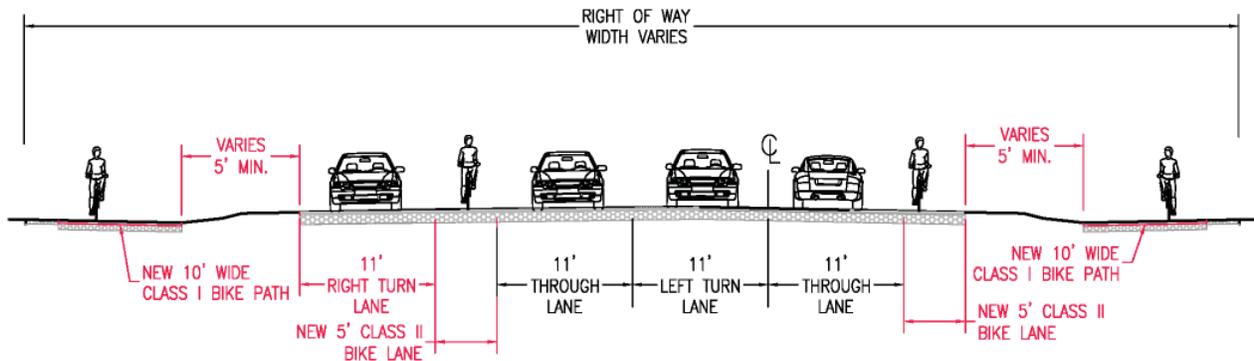


FIGURE 5-4 SOARING WAY - WEST OF JOERGER DRIVE

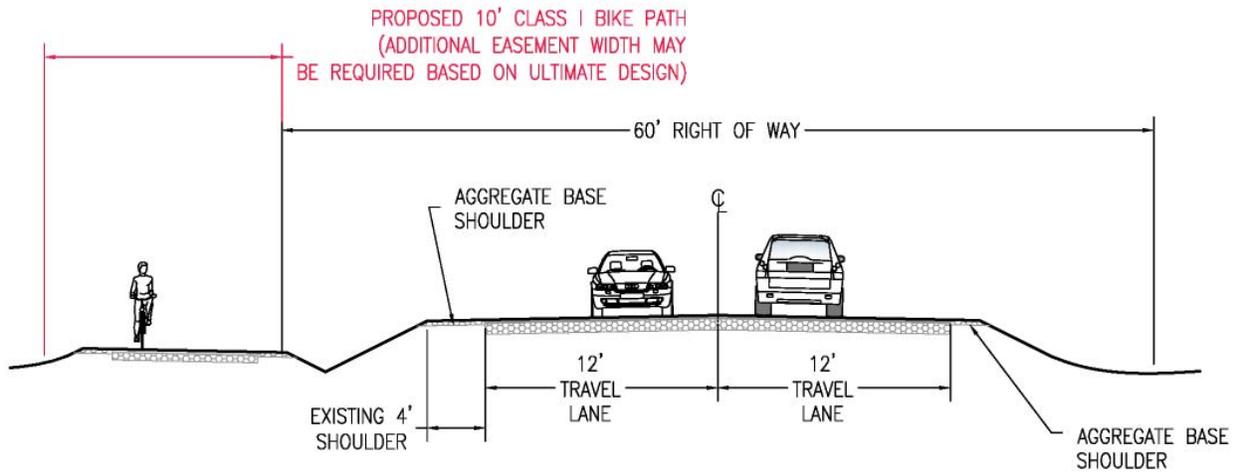


FIGURE 5-5: HOPE COURT

**HOPE COURT:** Hope Court currently consists of two 16' wide travel lanes and aggregate base shoulders 2' - 4' in width. The proposed roadway section for Hope Court illustrated in Figure 5-5 narrows the road width to 12' lanes and includes the addition of a detached 10' wide Class I bicycle path that will continue to the east and connect to the Martis Valley Regional Trail system. The Class I Bike Trail may be located on either side of Hope Court, depending on final design.

**MARTIS DRIVE:** Martis Drive is currently a narrow dirt road and will be constructed to meet Town road standards, as conceptually illustrated in Figure 5-6. The final location of Martis Drive

depends on a number of factors not determined at the time of Specific Plan preparation including the outcome of the Corridor Plan and project-specific site design, land use, and traffic volumes. Generally, if the road is constructed in its current location, a detached Class I bike path will be required on the westerly side all the way to the northerly limits of Joerger Ranch. In the event Martis Drive is required to be constructed elsewhere within the Plan, future development will require the construction of a 10' Class I bike path from Brockway Road to the residential neighborhood north of the BIZ zone. Additional right-of-way or a public trails easement will be required to be granted by future developers, if necessary, to fit the final Class I trail alignment.

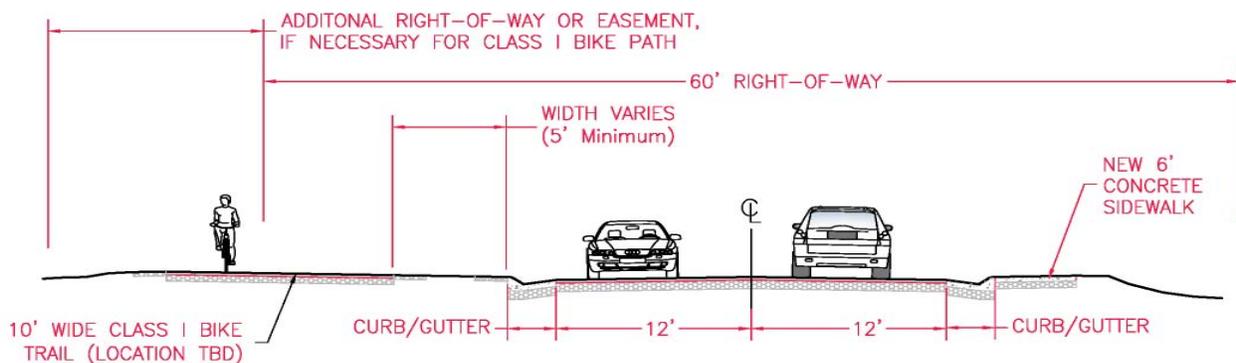


FIGURE 5-6: MARTIS DRIVE

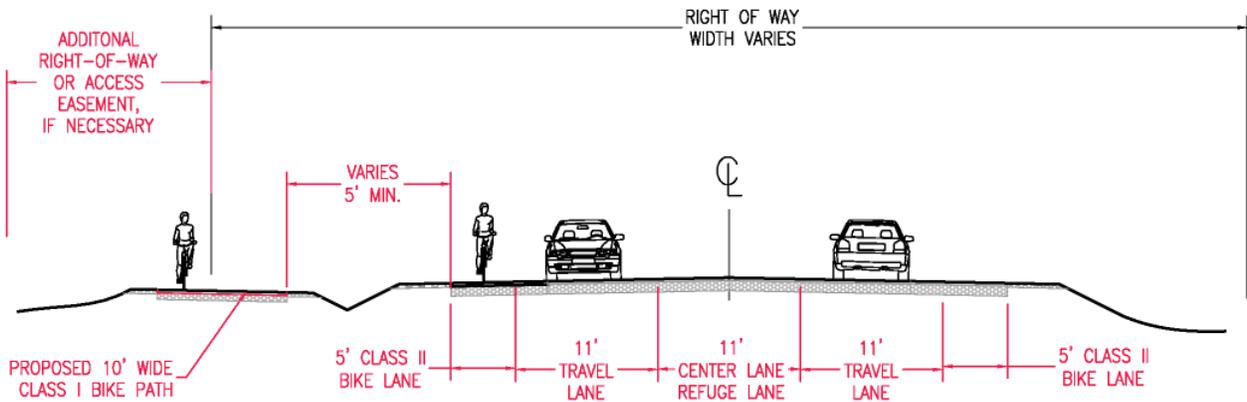
**BROCKWAY ROAD:** The existing Brockway Road consists of two 11' wide travel lanes, a center turn/refuge lane and two 5' wide Class II bicycle lanes.

Ultimately, the Brockway Road improvements will be determined upon review of a specific development plan for proposed Parcel 4, consisting of all Joerger Ranch property west of Highway 267. Joerger Ranch Policy P4.4 limits the continued use of a two-stage left turn lane except where there are low turning movement volumes.

Brockway Road, west of Hope Court, could remain primarily the same with the addition of a Class I bicycle path on the northerly side of Brockway Road from Martis Drive and crossing Brockway Road to Hope Court. Additional improvements

such as an additional pass-through lane, center median, and/or relocation of Martis Drive may be necessary depending on the ultimate build-out of Parcel 4. Construction of a roundabout at Brockway Road / Hope Court intersection may also be required if Martis Drive is relocated.

Brockway Road, east of Hope Court intersection, is illustrated in Figure 5-8 and will transition as it approaches Highway 267 to accommodate a westbound through lane, designated left turn lane, northbound through lane, designated right turn lane and two 5' wide Class II bike lanes. Additionally, a 10' wide Class I bike path is proposed for a safe pedestrian approach to Highway 267. A different configuration will be required if a roundabout is constructed at SR 267.



5-7: BROCKWAY ROAD - WEST OF HOPE COURT

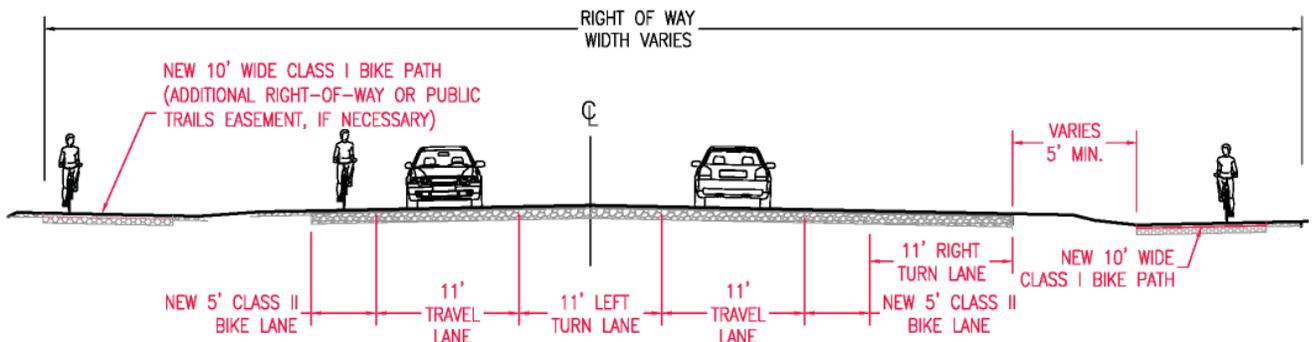


FIGURE 5-8: BROCKWAY ROAD - EAST OF HOPE COURT

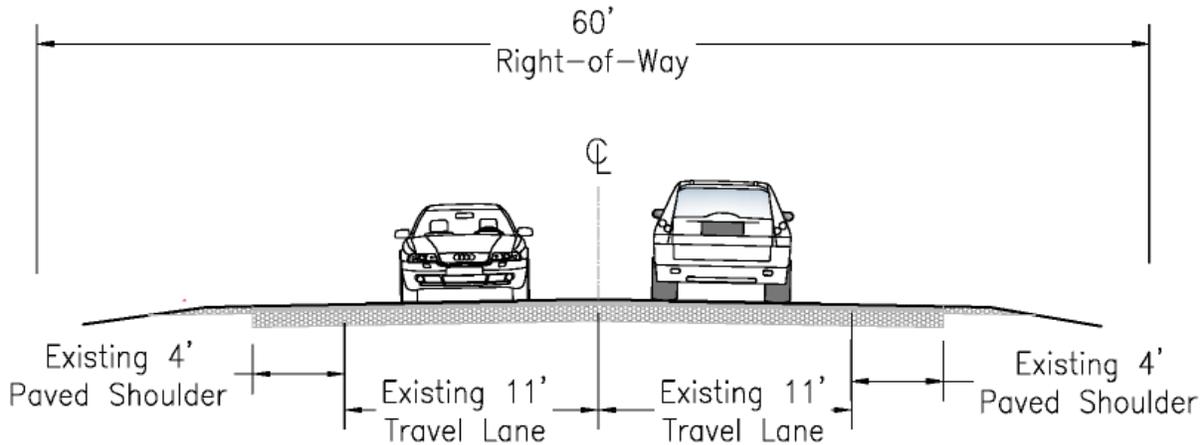


FIGURE 5-9: JOERGER DRIVE

**JOERGER DRIVE:** The proposed Joerger Drive cross-section, as illustrated in Figure 5-9, will maintain two 11' wide travel lanes and 4' shoulders. A 10' wide Class I bike path will be constructed through the open space and continue off-site to the Riverview Sports Park (see Figure 5-10).

### **HIGHWAY 267 INTERSECTION**

The existing intersection at Brockway Road / Soaring Way / Highway 267 is currently signalized with northbound and southbound through lanes with additional left turn lanes onto Soaring Way from the north and onto Brockway Road from the south. Traffic from Brockway Road approaches a through/left turn lane and a designated right turn lane. There is one (1) eastbound through lane to Soaring Way and one (1) westbound lane approaching from Soaring Way. Improvements to this intersection have been identified in the General Plan as a “future community need” independent of the traffic impacts resulting from the Joerger Ranch Specific Plan. Improvements to this intersection are in the Town’s Capital Improvement Program and ultimate design will be determined by the Town Engineer. Each development within Joerger Ranch is required to pay “fair share” traffic impact fees prior to building permit issuance.

On the Brockway Road side of the intersection, widening of the roadway is proposed in order to shift the designated right turn lane to the South and accommodate an additional lane. This additional lane will allow the existing through / left turn lane to be separated into a designated left turn only and designated through lane. Class II bike lanes are proposed in both easterly and westerly directions. A Class I bike path is proposed on the south side of Brockway Road to facilitate pedestrian safety up to the signalized intersection. The north side of Brockway Road will include a Class I bike path which will cross Highway 267 and continue toward the north to Riverview Sports Park. Final alignment may be modified by the Town Engineer.

On the Soaring Way side of the intersection, widening is proposed to allow for a designated right turn lane with a through/left and eastbound lane. Signalization upgrades along with lane widening is proposed to accomplish these intersection upgrades.

### **BROCKWAY ROAD / HOPE COURT**

The existing three-leg “T” intersection at Brockway Road and Hope Court is anticipated to remain, however this intersection will be reevaluated upon submittal of a development plan for Parcel 4 and

additional improvements may be necessary. At a minimum, striping and minor widening will be required to create a left turn pocket west bound on Brockway Road onto Hope Court. Alternatively, a roundabout may be required per Policy P4.5.

A pedestrian and bicycle crossing for the Class 1 bike path will be located at or near the Brockway Road / Hope Court intersection. The final location of the crossing will be evaluated and determined during the review of development west of Highway 267 and the required Brockway Road corridor study. Safety measures for bicyclists and pedestrians will be incorporated into the crossing and shall be reviewed & approved by the Town Engineer. As part of any parcel or final map that includes the triangle piece along Hope Court, a “no access” easement shall be recorded to prohibit direct access onto Brockway Road.

#### **BROCKWAY ROAD / MARTIS DRIVE**

This intersection currently serves as the primary access to the existing winery. The ultimate location of Martis Drive will depend on the future land uses within Parcel 4. If Martis Drive is constructed in its current location, improvements will be made to this intersection including but not limited to striping within Brockway Road, curb and gutter on Martis Drive and a Class 1 bicycle path crossing. Further evaluation of this intersection will be conducted upon a specific development application for Parcel 4 and additional improvements may be required.

#### **SOARING WAY / JOERGER DRIVE:**

Currently, Soaring Way is uncontrolled with both an eastbound and westbound lane. The Soaring Way / Joerger Drive intersection will be improved to a single lane roundabout (see Chapter 7 - Implementation & Phasing for details).

## **2.2 Intersection Improvements**

Major intersection improvements are further discussed in Chapter 7 -Implementation & Phasing. Depending on the timing of development within the planning area, the existing intersections at Joerger Drive / Soaring Way and Hwy 267/Brockway Road/Soaring Way may have operational capacity to accommodate some level of development without need for major improvement as set forth within this Chapter and Chapter 7. Should the Town pursue a roundabout at Hwy 267/Brockway Road/Soaring Way through the Towns Capital Improvement Program, timing of that improvement could result in the intersection improvement recommended in this Specific Plan to not be warranted or to be removed. Interim improvements may be considered during the site specific development review process for individual development permits. Such consideration may be given on a case by case basis and subject to the circumstances in effect at the time.

Reasonable interim improvements can be proposed to allow areas within the plan to develop incrementally. However, it is required that each development pay their proportional fair share of the common infrastructure improvements required by this plan. When an individual project necessitates construction of the common improvements and/or approved interim improvements, the developer of that individual project will be responsible for either posting financial security for the estimated cost of the improvements or construction and completion of those improvements. The project developer can record a reimbursement agreement and be permitted to collect the proportional fair share monies from future development projects within the Joerger Ranch Specific Plan area in order to compensate for expenditures to meet the overall common area improvements obligations of this plan.

### 3 | Bicycle Network & Design

A 10 foot wide separated Class 1 bicycle path is proposed to connect all four quadrants of the Joerger Ranch Specific Plan and also connect to the Martis Valley Regional Trail system and Riverview Sports Park (see Figure 5-10). A Class 1 bicycle path is also proposed to be constructed on the westerly side of Martis Drive to the northern property boundary of the Joerger Ranch Plan Area which will allow for a future extension to connect to the Legacy Trail to the

north. The conceptual location is shown in Figure 5-10 and throughout this Specific Plan, however the ultimate location will depend on where Martis Drive is constructed.

Class II bicycle lanes are also integrated into the various roadway sections including Brockway Road and Soaring Way.

The timing of construction of Class I bike paths will depend on which parcel develops as outlined below although construction may be required to occur earlier at the discretion of the Town Engineer.



FIGURE 5-10: TRAILS EXHIBIT

The Town Engineer may approve alternative equivalent trail segments that meet the intent of the Specific Plan and achieve a connected trail network within the Specific Plan Area and to existing adjoining regional trails.

Trail Segment	Required Prior to Subdivision of or Development in Parcel?			
	Parcel 1 (CR, M1)	Parcel 2 (M1)	Parcel 3 (OS)	Parcel 4 (BIZ, RMW-20, CL, OS)
Martis Drive Class I	No	No	No	Yes
Joerger Drive Class I and off site connection to Riverview Sports Park	Yes	Yes	No	No
Brockway Road Class I	Yes	Yes	No	Yes
Soaring Way Class I (SR 267 to Joerger / Soaring Roundabout)	Yes	Yes	No	No
SR 267 Class I	Yes	No	No	No
Hope Court Class I and off site connection to SR 267	No	No	No	Yes

FIGURE 5-11: TRAIL CONSTRUCTION SCHEDULE

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## Chapter 6

### Public Services & Infrastructure



## 1 | Introduction

Several independent special service districts and private utility companies provide the range of needed utilities and infrastructure to Truckee, including Joerger Ranch. This Chapter outlines the general infrastructure and utility service program for Joerger Ranch to ensure that sufficient infrastructure is provided to, and within, the Plan Area.

This Chapter describes the existing infrastructure related to water, wastewater, storm water drainage, electric, gas, phone, cable tv and solid waste disposal in the immediate vicinity of the Joerger Ranch Plan Area, and a general outline of needed infrastructure

improvements to serve development within the Plan Area. A Utility & Infrastructure Plan (Figure 5-2) is included, depicting the existing utility locations and likely points-of-connection for water, sewer, telecommunications and electric service to the various subdistricts of the Plan Area. The Utility & Infrastructure Plan also indicates known on-site looped connections and improvements necessary to serve Joerger Ranch. The ultimate design of these improvements, including their location, size, and capacity, is reliant upon development proposals and coordination and approval with each of the respective service entities.

## 2 | Hydrology & Storm Drainage

### 2.1 Surface Water Features

The project is within the Truckee River watershed and is located approximately 0.75 miles south of the river. This portion of the watershed consists of meadows and intermittent channels bounded to the east by the Martis Creek watershed area and bounded to the south and west by sloping topography. Overall the size of the drainage area is approximately 2 square miles. Elevation of its ground surface ranges from about 5,800 feet above mean sea level (msl) along Truckee River to 6,760 at Bald Mountain, a mountain summit that defines the southwesterly extent of the watershed.

In general, the southerly half of the drainage is dominated by mountainous terrain and steep hillsides with intermittent channels and unnamed tributaries. At the base of the hillside area, there is flat terrain with woodlands and a series of meadows. The meadow areas are dispersed across a broad

area along Brockway Road, Highway 267 and the Truckee-Tahoe Airport. In many cases the meadow areas are isolated with no surface connection to Truckee River. The project site is located on the northern half of the drainage shed area on the relatively flat terrain with grasslands, meadows and woodlands.

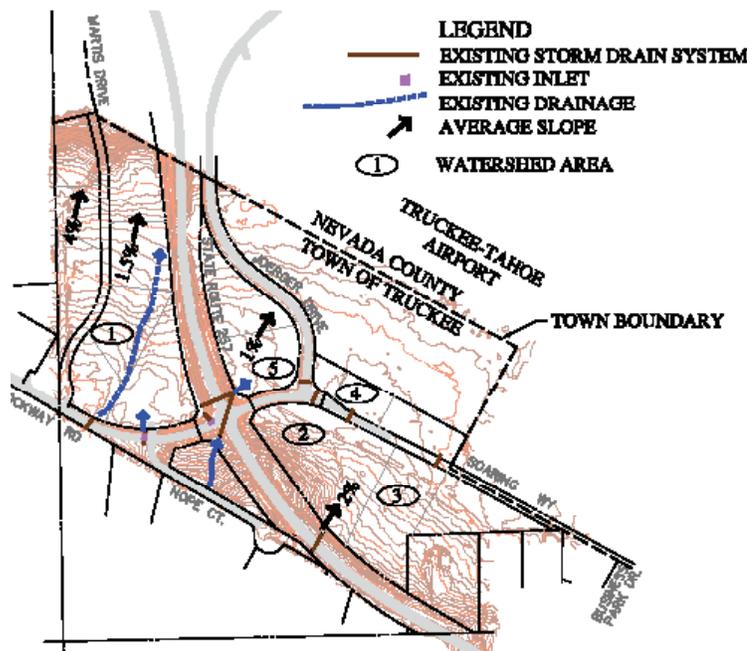


FIGURE 6-1

Surface runoff is conveyed primarily as sheet flow through the project site in a northerly direction toward Truckee River. The site contains two intermittent ephemeral drainage courses that are fed from the south by culverts passing under Brockway Road and State Route 267.

## **2.2 Hydrogeologic Information**

The geological setting for Martis Valley Basin (and the project site) consists of plutonic/metamorphic rock, sedimentary deposits and volcanic deposits. Groundwater level elevations are controlled by the complex stratification of the hydrogeologic units, topographic relief, and groundwater flow barriers. Groundwater in the basin is largely fed by precipitation, snow melt and seepage from the mountainous terrain to the southwest. The underlying Martis Valley aquifer is the main water supply for the Town of Truckee and other public and private entities.

Average precipitation is estimated to be 23 inches in the lower elevations of the northern portion of the basin to nearly 40 inches in the southern areas. Approximately 75 percent of the annual precipitation is received in the winter and early spring months as snow or mixed rain and snow. Precipitation throughout the remainder of the year primarily consists of thunderstorms.

## **2.3 Project Drainage**

The Joerger Ranch project drainage is defined by five drainage shed areas shown in Figure 6-1.

Surface Water and drainage will be managed through a combination of natural and built features to conserve water quality, natural hydrology and habitat and preserve biodiversity through conservation of water bodies and wetlands. Low Impact Development (LID) stormwater management strategies will be used to maintain the natural hydrologic function of the site with localized small-scale source control techniques that disperse flows and manage runoff close to where it originates. Storm drainage from impervious areas

(roads, walks, roofs) will be collected and routed through water quality treatment facilities designed to reduce the rate of runoff to pre-project conditions, remove potential pollutants and facilitate infiltration.

LID Drainage facilities may include Water Quality Inlets; Buffer Strips; Soil Amendments; Earthen swales; Rock-lined swales; Bioswales; Rock Infiltration Basins; Retention Basins and Slope Stabilization. These facilities will be used to capture sand and sediment, provide filtration of pollutants and allow infiltration to underlying soils. Grading disturbance shall be minimized by use of construction fencing. All graded disturbance areas shall be restored with soil stabilization and natural vegetation through use of organic material such as wood chips, mulch, and native plantings for protective ground cover.

Per the guidelines of the State of California, Water Resources Control Board, Lahontan Regional Water Quality Control Board (SWRCB-Lahontan) and as required by Town of Truckee, onsite retention shall be provided for 20-year, 1-hour volume of water from impervious areas. This equates to the first 0.7 inches of rainfall from all impervious surfaces including roadways, sidewalks, paved paths, roofs and driveways. Peak discharge flows are reduced to rates and quantities at or below existing pre-project



FIGURE 6-2 SR 267 CULVERT

conditions by use of onsite retention facilities that attenuate flows and provide infiltration.

Snow storage and snow removal are important considerations on-site and within the adjacent public road and highway rights-of-way. Snow will be stored on-site in landscape areas and other undeveloped areas. If the required amount of snow storage cannot be handled on-site, it will be hauled off-site to a permitted disposal site. Storm water runoff from snow storage areas will be routed through water quality treatment facilities prior to discharge. Snow removal shall be further described in a Maintenance Agreement between property owner and the Town. Prior to recordation of Phase I Parcel Map, a Drainage Master Plan is required to define the drainage facilities and easements (Town Council Resolution No. 2015-09).

## **2.4 Water Quality**

Water quality controls for the project shall be implemented in compliance with local, State and Federal requirements. California State Water Resources Control Board (SWRCB) enforces the federal Clean Water Act and Porter-Cologne Water Quality Control Act, including administration of National Pollutant Discharge Elimination System (NPDES) permits. Lahontan Regional Water Quality Control Board (LRWQCB) has surface water and groundwater objectives, prohibitions and guidelines as set forth in the Water Quality Control Plan for the Lahontan Region (Basin Plan).

During construction, Best Management Practices (BMP's) including temporary erosion control facilities shall be implemented to control any pollutants that could potentially affect the quality of storm water discharges from the site. A Storm Water Pollution Prevention Plan (SWPPP) will be prepared and the project will comply with the State of California's General Permit for Storm Water Discharges Associated with Construction

and Land Disturbance Activities. This includes implementation, maintenance and inspection of non-storm water and storm water discharge; erosion and sediment control measures; materials and waste management; general site clean-up; vehicle and equipment cleaning, fueling and maintenance; and spill control ensuring that no materials other than storm water are discharged in quantities which will have an adverse effect on receiving waters.

Post Construction BMP's will also be implemented in accordance with NPDES Stormwater Phase II and Town of Truckee's municipal separate storm sewer system (MS4) requirements. Post Construction BMP's include runoff control measures, water quality facilities, operations and maintenance program, employee training, recycling and waste disposal program and public education (signage/brochures) for storm water protection. Permanent water quality facilities that remain in place upon completion of the project such as bio-swales, retention basins and water quality inlet structures remove and filter potential common pollutants such as oil and grease from roadways, pesticides from lawns, sediment, and carelessly discarded trash prior to discharge of storm water to natural water course.

## **3 | Sanitary Sewer**

Wastewater collection and conveyance is provided by the Truckee Sanitary District (TSD). Wastewater treatment is provided by the Tahoe-Truckee Sanitary Agency (T-TSA). Sewage is currently collected primarily by gravity flow throughout adjacent developed areas, and is transported in a sewer main line at Joerger Drive for conveyance to the treatment plant located east of the Town of Truckee.

Figure 6-3 shows the points of connection and the mainline extensions needed to serve this project. The on site sewer line layouts in each sub-District are shown with dashed lines. Final sewer collection

system layouts and pipe sizes will depend on the building layouts and will comply with TSD design requirements.

extending the existing 6” sewer stubs along Soaring Way. Final mainline extensions and on site system layouts will comply with TSD design requirements.

Each Specific Plan area shall provide easements as required by TSD for the public sanitary sewer improvements and access thereto.

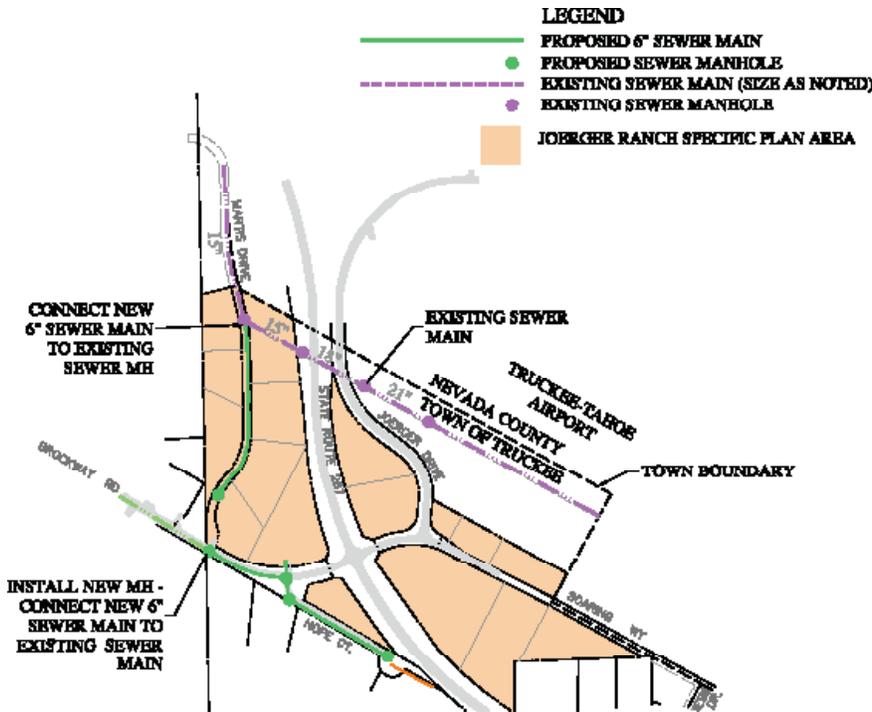


FIGURE 6-3

Figure 6-3 also details the existing sewer mainline and the proposed mainline extensions. Each sub-District may build its own stand alone mainline extension and connection, independent of the order of phasing, as described herein. The Neighborhood District shall connect to the existing 21” sanitary sewer near its northern boundary. The RM-F zoning area at the intersection of Hope Court / Brockway Road shall connect to the existing sanitary sewer in Brockway Road and extend it east along Hope Court. The elevation of the existing point of connection will determine how much of the Hope Court site can be served by gravity sewer and whether or not service pumps are required. Development along Joerger Drive shall connect to the existing 21” sanitary sewer at its north boundary. A mainline extension within Joerger Drive is not necessary. The remainder of the CRS Zoning area would be required to connect on site by

## 4 | Water

### 4.1 Water Supply

Water service in Truckee is provided by the Truckee Donner Public Utility District (TDPUD), a publicly owned utility providing electric and water service since 1927. The District operates three water systems in the Truckee area: the Hirshdale System, the Truckee

System, and the Donner Lake System. The Truckee System serves the Joerger Ranch Plan Area.

Existing transmission, distribution and treated water storage facilities will serve both existing and future demand from the planned development. This basic infrastructure has developed by TDPUD in accordance with the Water Master Water Plan and is sufficient to supply the project. Water mainlines are located within the adjacent roadways and will be extended throughout the project site for domestic water distribution and fire suppression.

The project water demand is equivalent to, or less than, the amount of water required per the General Plan and TDPUD Water Master Plan. Water consumption will be reduced in accordance with California Green Building Standards Code (CalGreen) water saving mandates through the use of water efficient fixtures, fittings, flow restrictors and irrigation controllers.

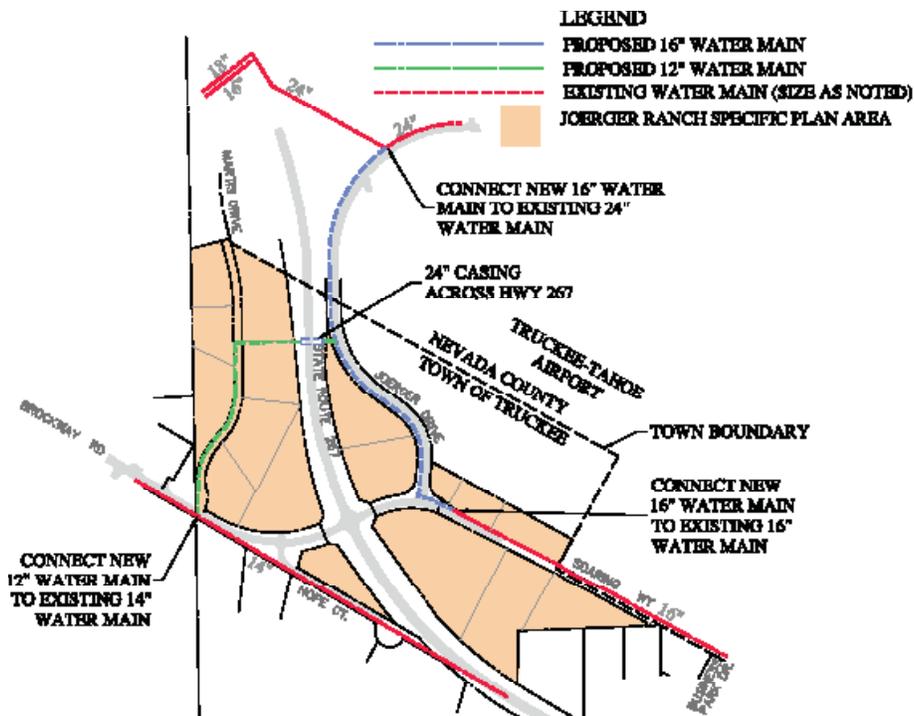


FIGURE 6-4

**4.2 Existing System**

Figure 6-4 shows the TDPUD water distribution system in the vicinity of this project. Existing water main sizes are shown.

**4.3 Proposed System**

Figure 6-4 also shows the points of connection and the water mainline extensions needed to serve this project. The layout of these mainline extensions will comply with the TDPUD Water System Master Plan and meet their approval. The on site water line layouts in each Specific Plan area are shown with dashed lines. Final water line layouts and pipe sizes will depend upon the results of hydraulic analysis that take into account existing system pressures, project elevations, fire flow requirements and domestic demands. Hydraulic analysis will be performed with the existing TDPUD hydraulic model, in accordance with their design requirements.

**5 | Energy**

**5.1 Electric Service**

The Joerger Ranch Project lies within the service area of Truckee Donner Public Utility District (TDPUD). Existing electrical transmission lines and service distribution lines lie adjacent to and within the Plan Area. Electrical service facilities will be extended from existing TDPUD infrastructure and shall be upgraded as necessary to adequately serve the project as it develops and shall be designed to accommodate

ultimate development at build out. These facilities shall be designed and extended as directed by TDPUD and in accordance with TDPUD and California Public Utilities Commission (CPUC) rules. Common trench utilities including electric, natural gas, telephone, and cable TV services shall be located underground within public utility easements. Placement of transformer boxes shall be coordinated with TDPUD. All utility lines necessary to serve Joerger Ranch development shall be located underground.

**5.2 Natural Gas**

Natural gas service is provided to the Truckee area by Southwest Gas Corporation. Existing natural gas transmission lines and service distribution lines lie adjacent to and within the Plan Area. Natural gas facilities will be extended from existing Southwest infrastructure in Martis Drive and shall be upgraded as necessary to adequately serve the project as it develops and shall be designed to accommodate maximum build out. These facilities shall be designed and extended as directed by

Southwest Gas and in accordance with Southwest and California Public Utilities Commission (CPUC) rules. Natural gas lines will be included within the common trench utilities which shall be located underground within public utility easements. Placement of gas service meter locations shall be coordinated with Southwest Gas.

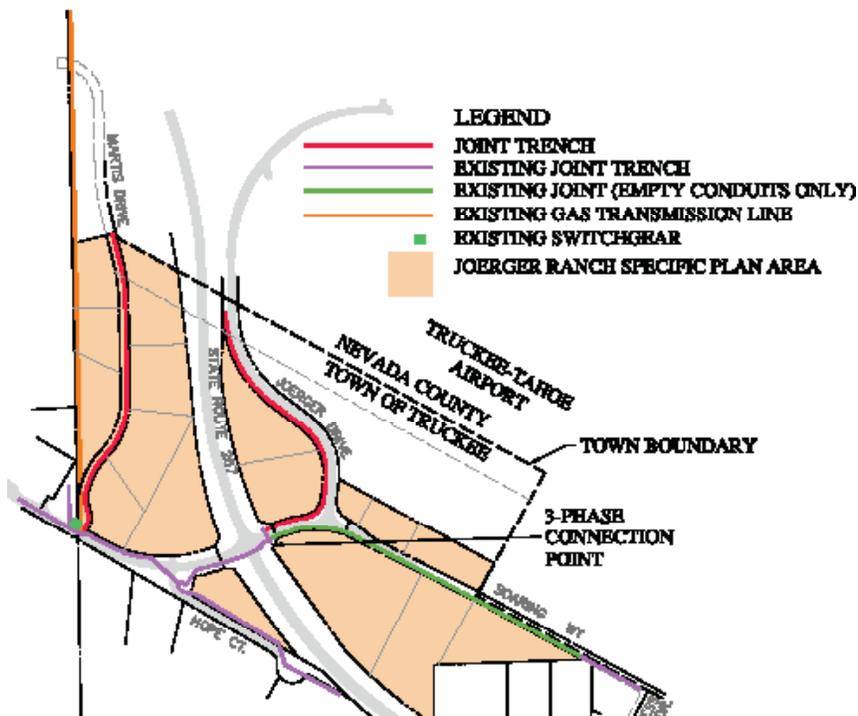


FIGURE 6-5

## 6 | Telecommunications

Telecommunications service is provided by AT&T. Cable TV is provided by Suddenlink Communications. Satellite TV is also available. Telecommunications service facilities will be extended from existing infrastructure and shall be upgraded as necessary to adequately serve the project as it develops. These facilities shall be designed and extended as directed by telecommunications providers and in accordance with their rules. All utilities including electric, natural gas, telephone, and cable TV services shall be located underground.

## 7 | Law Enforcement & Fire Protection

### 7.1 Law Enforcement

Police service in Truckee is provided by the Truckee Police Department. The Police Department operates out of its headquarters at 10183 Truckee Airport Road on the easterly terminus / intersection with Soaring Way and less than a ¼ mile from the Plan Area. All calls are responded to from the Department’s headquarters, allowing for a fast response time to the Plan Area.

### 7.2 Fire Protection

The Truckee Fire Protection District (TFPD) provides fire prevention, fire suppression, emergency medical care and/or transportation, assorted rescue services, and public education services within the Town of Truckee.

Three TFPD fire stations are located in close proximity to the Plan Area: Station 91 in Downtown Truckee, Station 92 in the Gateway area, and

Station 96 in the Truckee Tahoe Airport area. Station 91 is located in downtown Truckee and houses the Fire Chief’s office, the District’s administrative staff and the Prevention Bureau; Station 92, in the Gateway area, is located next to the Truckee High School and in front of the Sierra Mountain Middle School. It is the District’s largest station with the most equipment.

The Town of Truckee lies within a high Fire Hazard Severity Zone, as defined by the California Department of Forestry, indicating that wildland fires are considered to pose a significant hazard. The level of fire danger risk within the town ranges from moderate to very high based on the density of development within a particular area and its proximity to forested areas.

## 8 | Solid Waste & Recycling

Solid waste removal and recycling services for the Town of Truckee are provided by the Tahoe-Truckee Sierra Disposal (TTSD) Company. Two separate bodies make up the TTSD: Tahoe Truckee Disposal (TTD) and the Eastern Regional Landfill Material Recovery Facility (MRF). TTD is responsible for collecting household waste and recyclables to the MRF. The MRF is the recycling center for household and construction materials that acts as a transfer station for household waste. Incoming solid waste is either recycled or transported to the Lockwood Regional Landfill in Storey County, Nevada. This 1,535-acre site has a 60-year capacity to accommodate the buildout projections for the TTSD's service area. Currently, TTSD is in its twelfth year of an 80-year contract for disposal services at the landfill.

TTD is the collection division of TTSD and operates close to 40 vehicles to collect waste. TTD uses a combination of rear mounting bin pick up trucks for single-family residences and low-density areas, and front loader garbage trucks for commercial and multi-family areas. Funding for solid waste collection comes from collection fees. TTSD handles approximately 60,000 tons of waste per year and is operating at 50 percent of their total capacity of 120,000 tons per year. In 2002, the capacity of the MRF facility and transfer station were increased by 100 percent. TTSD plans on continuing to expand their services to accommodate the growth and increasing needs of their service area.

## 9 | Mail Service

Mail delivery and collection is provided for the Town and surrounding region by the U.S. Postal Service (USPS) from a distribution facility located in Downtown Truckee. The project development will include "clustered" type mailboxes – including free-

standing, pedestal-mounted cluster box unit (CBU), or other cluster mailboxes mounted in a wall, kiosk, or shelter in accordance with USPS regulations and requirements.

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# Chapter 7

## Implementation & Phasing



## 1 | Introduction

This chapter is intended to identify the orderly development implementation and phasing of the Joerger Ranch Specific Plan infrastructure.

The Joerger Ranch Specific Plan area is divided by state and local roadways that create four separate and distinct quadrants. Market demand, economic conditions and financing will all factor into the incremental implementation and phasing of uses.

The overall Joerger Ranch Specific Plan has common infrastructure improvements that benefit all properties within the plan area. These improvements are needed to be constructed as major portions of the plan area develop. Interim or incremental improvements may be allowed by the Town Engineer on a case by case basis (see Chapter 5) depending on traffic generation from the proposed land use. Common infrastructure requirements are distributed among the three developable parcels of the Phase I Parcel Map (see Appendix E). Because no development is proposed concurrent with this Specific Plan adoption, the infrastructure requirements are structured to ensure adequate road, utility, bike lane, Class I trail, drainage improvement and snow storage areas are provided or constructed as the plan builds out incrementally. The specific requirements are summarized below by Parcel 1, Parcel 2, and Parcel 4. Parcel 3 will be preserved as open space. Since no development is proposed within Parcel 3, there are no infrastructure requirements tied to the creation of this parcel.

## 2 | Infrastructure Requirements - Parcel 1

The first property owner to develop and/or subdivide within Parcel 1 shall construct the improvements identified on Figure 7-1 and outlined below. All future development shall be required to pay their “fair share” portion of actual infrastructure costs

and provide proof of payment to the Town prior to issuance of building permits or recordation of any parcel or final maps.

### 1. Highway 267 / Brockway Road / Soaring Way Intersection

a. **Highway 267** - Widen Highway 267 and construct northbound right turn lane, crosswalks and all necessary striping.

b. **Brockway Road** - Widen and construct right turn lane, left turn lane, east-bound and west-bound through lanes, 10-foot wide Class I bike path on north side of Brockway Road (from Highway 267 to the Specific Plan Area limits) and on the south side of Brockway Road to Hope Court, 5-foot wide Class II bike lanes on both sides, curb/gutter, and all necessary striping.

c. **Soaring Way** - Widen and construct left turn lane, right turn lane, west-bound & east-bound through lanes, 10-foot wide Class I bike paths on both sides of Soaring Way to Soaring/Joerger roundabout, Class II bike lanes on both sides of Soaring Way to Soaring/Joerger roundabout, and curb/gutter.

### 2. Soaring Way

a. Construct single-lane roundabout at Soaring Way and Joerger Drive intersection.

b. East of the roundabout, construct 6-foot wide sidewalk along property frontage. Restripe Soaring Way to include 5-foot wide Class II bike lanes on both sides of Soaring Way and on-street parallel parking on the south side of Soaring Way.

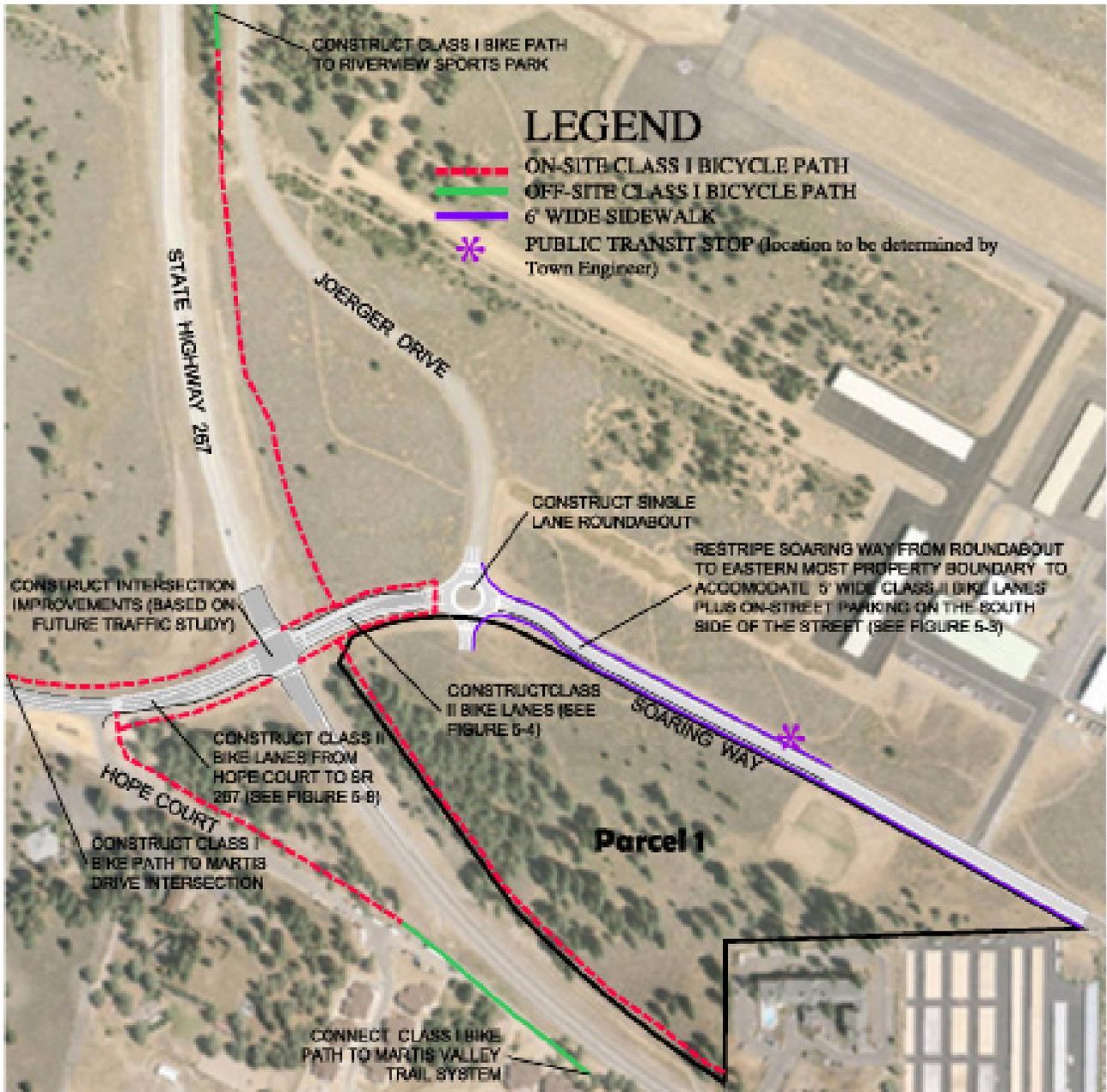


FIGURE 7-1: PARCEL 1 INFRASTRUCTURE REQUIREMENTS

If on-street parking along the north side of Soaring Way becomes necessary in the future, additional widening/stripping of Soaring Way will be the sole responsibility of Parcel 2.

c. Construct public transit stop (location to be approved by Town Engineer).

3. 10-foot Wide Class II Bike Paths (not otherwise discussed in this section)
  - a. Within Parcel 3 (open space parcel) from Highway 267 to Riverview Sports Park (final alignment to be approved by Town Engineer).

- b. Along Highway 267 within Parcel 1 boundary.
- c. Along Hope Court from Brockway Road to Martis Valley Trail System.

#### 4. Utilities

- a. **Sewer** - Connect to existing 6" sewer within Soaring Way and extend as necessary to serve development. Final layout shall be approved by TSD.
- b. **Water** - Extend 16" water main at Soaring Way to Joerger Drive and along Joerger Drive to the existing 24" water main near Riverview Sports Park. Final water line layouts and pipe sizes may change depending on the results of a hydraulic analysis and shall be approved by TDPUD.
- c. **Joint Trench (electric, gas, telephone, cable television)** - All utilities shall be located underground within existing empty conduits or installation of new conduits, as needed. Placement of transformer boxes shall be approved by TDPUD. Placement of gas service meter locations shall be approved by Southwest Gas.

### 3 | Infrastructure Requirements - Parcel 2

The first property owner to develop and/or subdivide within Parcel 2 shall construct the improvements identified in Figure 7-2 and outlined below. All future development shall be required to pay their "fair share" portion of actual infrastructure costs and provide proof of payment to the Town prior to issuance of building permits or recordation of any parcel or final maps.

#### 1. Highway 267 / Brockway Road / Soaring Way Intersection

- a. **Highway 267** - Widen Highway 267 and construct northbound right turn lane, crosswalks and all necessary striping.

- b. **Brockway Road** - Widen and construct right turn lane, left turn lane, east-bound and west-bound through lanes, 10-foot wide Class I bike path on north side of Brockway Road (from Highway 267 to the Specific Plan Area limits) and on the south side of Brockway Road to Hope Court, 5-foot wide Class II bike lanes on both sides, curb/gutter, and all necessary striping.

- c. **Soaring Way** - Widen and construct left turn lane, right turn lane, west-bound & east-bound through lanes, 10-foot wide Class I bike paths on both sides of Soaring Way to Soaring/Joerger roundabout, Class II bike lanes on both sides of Soaring Way to Soaring/Joerger roundabout, and curb/gutter.

#### 2. Soaring Way

- a. Construct single-lane roundabout at Soaring Way and Joerger Drive intersection.

- b. East of the roundabout, construct 6-foot wide sidewalk along property frontage. Restripe Soaring Way to include 5-foot wide Class II bike lanes on both sides of Soaring Way.

If on-street parking along the north side of Soaring Way becomes necessary in the future, additional widening/stripping of Soaring Way will be the sole responsibility of Parcel 2.

c. Construct public transit stop (location to be approved by Town Engineer).

3. **10-foot Wide Class II Bike Paths** (not otherwise discussed in this section)

a. Within Parcel 3 (open space parcel) from Highway 267 to Riverview Sports Park (final alignment to be approved by Town Engineer).

4. **Utilities**

a. **Sewer** - Connect to existing 6” sewer

within Soaring Way and extend as necessary to serve development. Final layout shall be approved by TSD.

b. **Water** - Extend 16” water main at Soaring Way to Joerger Drive and along Joerger Drive to the existing 24” water main near Riverview Sports Park. Final water line layouts and pipe sizes may change depending on the results of a hydraulic analysis and shall be approved by TDPUD.

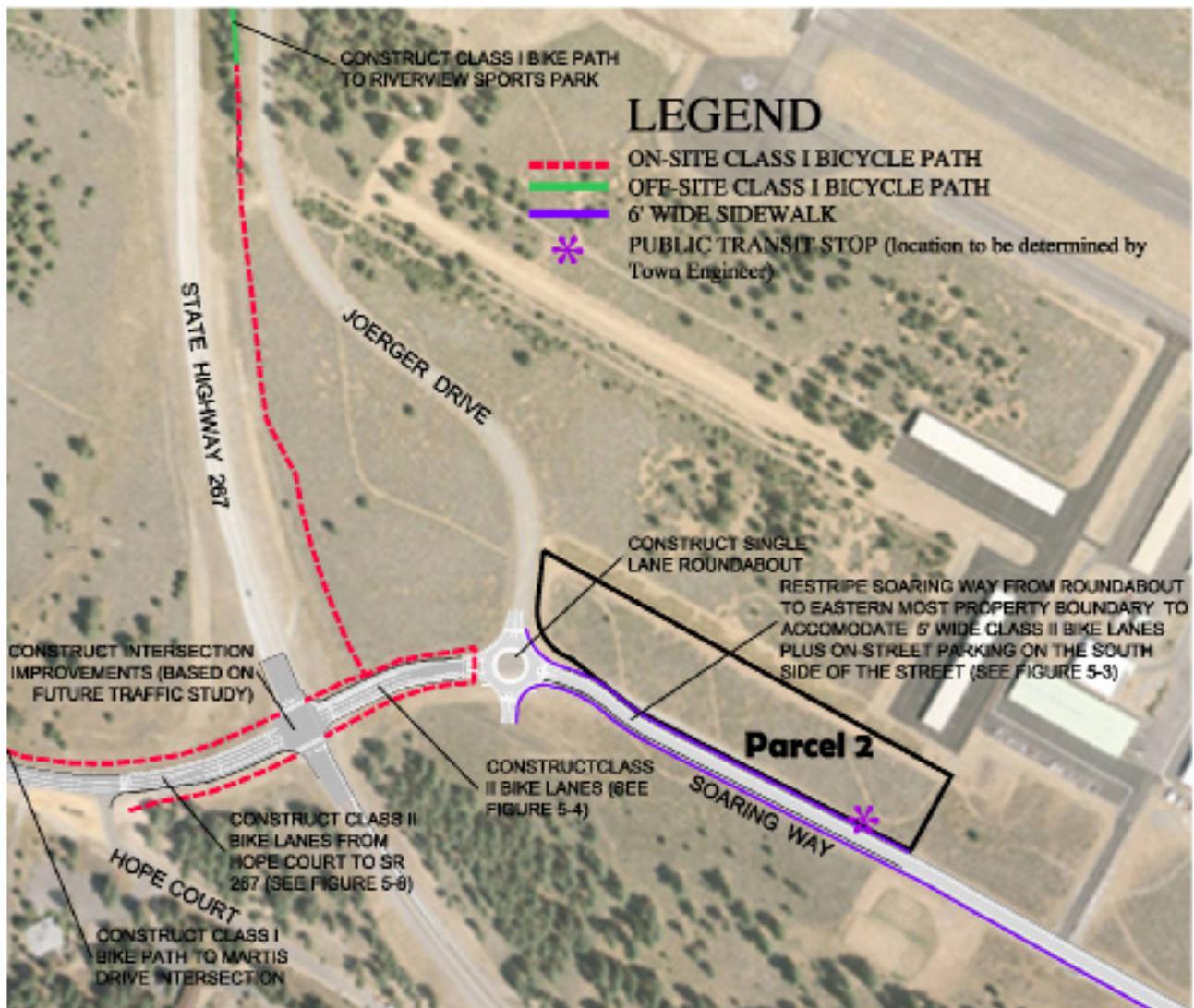


FIGURE 7-2: PARCEL 2 INFRASTRUCTURE REQUIREMENTS

**c. Joint Trench (electric, gas, telephone, cable television)** - All utilities shall be located underground within existing empty conduits or installation of new conduits, as needed. Placement of transformer boxes shall be approved by TDPUD. Placement of gas service meter locations shall be approved by Southwest Gas.

#### 4 | Infrastructure Requirements - Parcel 4

The first property owner to develop and/or subdivide within Parcel 4 shall construct the improvements identified in Figure 7-3 and outlined below. All future development shall be required to pay their “fair share” portion of actual infrastructure costs and provide proof of payment to the Town prior to issuance of building permits or recordation of any parcel or final maps.

Prior to development within Parcel 4, a Brockway Road Corridor Study shall be prepared. Based on the study, the Town Engineer will determine the location of Martis Drive, the level of frontage improvements necessary along Brockway Road, and the intersection improvements at Martis Drive/Brockway Road and Hope Court/Brockway Road.

##### 1. Highway 267 / Brockway Road / Soaring Way Intersection

**a. Highway 267** - Widen Highway 267 and construct northbound right turn lane, crosswalks and all necessary striping.

**b. Brockway Road** - Widen and construct right turn lane, left turn lane, east-bound and west-bound through lanes, 10-foot wide Class I bike path on north side of Brockway Road (from Highway 267 to the Specific Plan Area limits) and on the south side of Brockway Road to Hope Court,

5-foot wide Class II bike lanes on both sides, curb/gutter, and all necessary striping.

**c. Soaring Way** - Widen and construct left turn lane, right turn lane, west-bound & east-bound through lanes.

##### 2. Brockway Road

**a.** Streetscape for Brockway Road between the western-most Specific Plan Area boundary to Highway 267 for full Parcel 4 buildout including driveway location(s), intersection improvements for Martis Drive and Hope Court, and Class I trail crossing from the north side of Brockway Road to Hope Court.

**b.** Construct public transit stop (location to be approved by Town Engineer).

##### 3. Martis Drive

**a.** Construct Martis Drive (no gate/unrestricted access, two 12-foot travel lanes) and a 10-foot wide Class I bike path from Brockway Road to existing Martis drive located at the northern Specific Plan Area boundary.

##### 4. Hope Court

**a.** Narrow roadway to two 12-foot travel lanes and construct a 10-foot wide Class I bike path from Brockway Road to Martis Valley Trail.

##### 5. Utilities

**a. Sewer** - Connect to existing 15” sewer within Martis Drive and extend as necessary to serve development north of Brockway Road. Connect to existing sewer within Brockway Road and extend as necessary

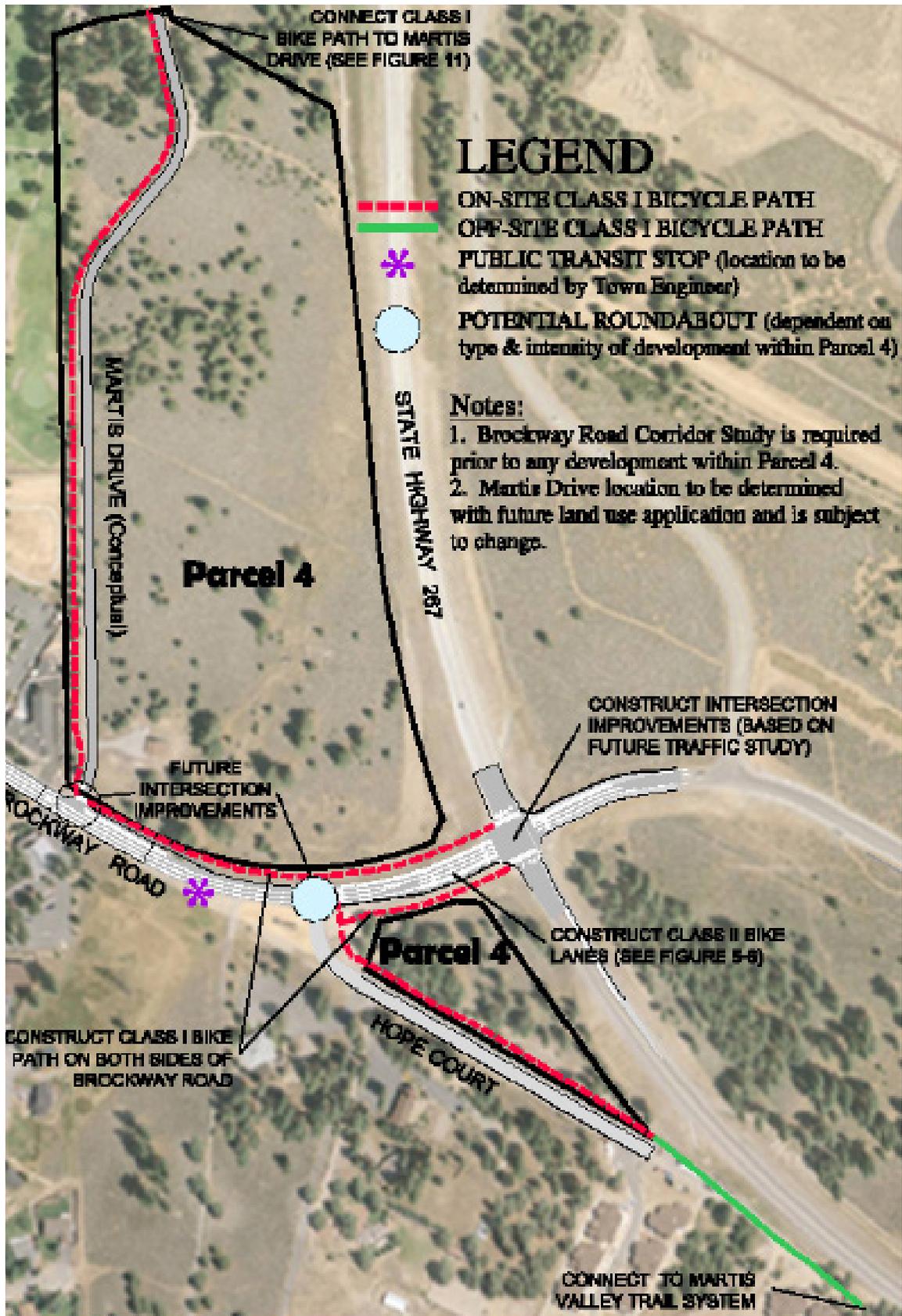


FIGURE 7-3: PARCEL 4 INFRASTRUCTURE REQUIREMENTS

along Brockway Road and Hope Court to serve development along Hope Court. Final layout and pipe sizes shall be approved by TSD.

**b. Water** - Extend 12” water main at across SR 267 through the existing 24” casing and along Martis Drive. Connect new water main to existing 14” water main at Brockway Road. Final water line layouts and pipe sizes may change depending on the results of a hydraulic analysis and shall be approved by TDPUD.

**c. Joint Trench (electric, gas, telephone, cable television)** - All utilities shall be located underground within existing empty conduits or installation of new conduits, as needed. Placement of transformer boxes shall be approved by TDPUD. Placement of gas service meter locations shall be approved by Southwest Gas.

The existing winery is a “legal non-conforming use” which will allow the winery to continue its current operation without being subjected to the infrastructure requirements as outlined above. If the winery use is discontinued, expands or substantially changes its operations or characteristics, the new use would be subject to the provisions of this Specific Plan.

The non-profit center proposed on Hope Court would be solely responsible for and only subject to the utilities, roadway, and Class I bike path improvements along Hope Court and intersection improvements at Hope Court / Brockway Road, if required.

At the Town Engineer’s discretion, some frontage improvements may be delayed if it is determined that the infrastructure is not required for specific

development and a plan to phase improvements has been submitted, reviewed, and approved by the Town Engineer.

Prior to issuance of a building permit for future projects, an offer of dedication for all necessary frontage and intersection improvements shall be completed.

## 5 | Workforce / Inclusionary Housing

The 2007-2014 Housing Element requires that all new residential and non-residential projects provide affordable housing to meet Truckee’s housing needs. General Plan Housing Element program H-1.2.3 requires that all new residential development allocate at least 15 percent of the units as affordable to very-low, low-, and moderate-income households. For non-residential projects that create jobs, Housing Element program H-1.3.4 requires the construction of affordable housing for at least 50 percent of the projected very low-, low- and moderate-income workforce.

To ensure General Plan consistency, approximately 4.0 acres of the Joerger Ranch Specific Plan area is zoned RMW-20 which allows multi-family residential at a minimum density of 18 units/acre and maximum density of 20 units/acre. This equates to a minimum of 72 dwelling units and a maximum of 80 dwelling units. The RMW-20 zone combined with payment of in-lieu fees (see Table 7.4B) is intended to capture the workforce housing needs of all future buildout of the Specific Plan. Development Code Chapters 18.214 (Inclusionary Housing) and 18.216 (Workforce Housing) shall not apply to projects in the RMW-20 zone or projects approved prior to the adoption of Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments. The construction of housing outside of the RMW-20

zone is permitted (airport restrictions excluded) but is not required.

Joerger Ranch has several site constraints that limit compatible locations for new residences (ie. airport restrictions, highway location, drainage areas, etc.). As an incentive for one developer to construct a single housing project on the RMW-20 zoned parcel, future development on the RMW-20 site will not be required to provide inclusionary housing and deed restrictions will not be required. Additional information about the RMW-20 parcel is discussed in Chapter 4 (Residential Multi-family RMW-20 Zoning District).

The Joerger Ranch workforce housing requirement/calculations are shown in Tables 7.4A and 7.4B. Based on a Floor Area Ratio (FAR) of 0.25 and using the employee generation assumptions in Development Code Chapter 18.216 (Workforce Housing), jobs would be created for approximately 578 full-time equivalent employees, if maximum buildout was achieved. This equates to a demand of approximately 82.7 workforce housing units. The minimum requirement within the RMW-20 zoning district is 72 workforce housing units and can accommodate up to 80 workforce housing units, excluding any density bonuses permissible by Development Code Chapter 18.212 (Density Bonuses, Concessions and Incentives). The demand of 82.7 units and minimum requirement to construct 72 units in the RMW-20 zoning district results in an “unmet demand” of 10.6 units under a full build-out scenario.

To ensure General Plan consistency, the Town

Council required both the construction of workforce housing units in the RMW-20 zone and payment of in-lieu fees. The in-lieu fee calculation is based on the unmet demand of 10.6 units and is required to be paid prior to the issuance of the first certificate of occupancy in each zoning district. The workforce housing construction and in-lieu fee requirements in Tables 7.4A and 7.4B apply to projects approved prior to the adoption of Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments. Future projects shall comply with the requirements of Development Code Chapters 18.214 (Inclusionary Housing) and 18.216 (Workforce Housing). Construction of workforce housing units is required on the project site or at an off-site location within the Truckee town limits at a location with comparable to transit and similar services, as well as comparable pedestrian and bicycle access. Construction of the off-site units must be completed prior to issuance of temporary or final certificate of occupancy for the commercial and/or manufacturing/industrial project. Payment of in-lieu fees or approval of an alternate equivalent proposal to address the workforce housing demand of future projects shall not be allowed. If Truckee Tahoe Airport Land Use Compatibility Zone restrictions limit the ability of a developer to construct all of the required workforce housing units on-site, the developer may request approval to construct the units off-site.

TABLE 7.4A. Workforce Housing Calculations by Zoning District

Zoning District	Land Area within B1/B2 Airport Zone (Acres)	Land Area Outside Airport Zone (Acres)	Development Potential (0.25 FAR)	Full-Time Equivalent Employees	# of Workforce Housing Units
Regional Commercial (CR)	0	16.3	177,507	355	50.7
Lifestyle Commercial (CL)	0	4.7	51,183	102	14.6
Manufacturing/Industrial (M1)	1.0	8.5	92,565	93	13.3
Business Innovation Zone (BIZ)	8.6	2.6	28,314	28	4.0
Multi-family Residential (RMW-20)	0	4.0	0	0	0
Open Space (OS)	6.8	14.1	0	0	0
Total	16.4	50.2	349,569 SF	578	82.6

**Note:**

1. The number of required workforce housing units is based on the maximum development potential with a floor area ratio of 25%. Land area that is zoned Open Space plus land area that falls within the B1/B2 Airport Zone are not included in the “development potential” calculations.
2. The workforce housing requirements identified in Table 7.4A apply to projects that were approved prior to adoption of the Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments. Future projects approved within the Specific Plan area shall be subject to the requirements of Development Code Chapter 18.216 (Workforce Housing). Construction of the workforce housing units is required on the project site or at an off-site location within the Truckee town limits at a location with comparable access to transit and similar services, as well as comparable pedestrian and bicycle access. Construction of the off-site units must be completed prior to issuance of temporary or final certificate of occupancy for the commercial and/or manufacturing/industrial project. Payment of in-lieu fees or approval of an alternate equivalent proposal to address the project’s workforce housing demand shall not be allowed. If Truckee Tahoe Airport Land Use Compatibility Zone restrictions limit the ability of a developer to construct all of the required workforce housing units on-site, the developer may request approval to construct the units off-site.

**TABLE 7.4B. Workforce Housing In-Lieu Fees by Parcel (Based on % of Unmet Demand)**

Zoning District	# of Required Workforce Housing Units	% of Total Workforce Housing Requirement	# of Unmet Workforce Housing Units	In-Lieu Fee (Based on \$77,480 per unit)
Regional Commercial (CR)	50.7	50.7 / 82.6 = 61.4%	0.614 X 10.6 = 6.5 Units	\$503,620
Lifestyle Commercial (CL)	6.2	6.2 / 82.6 = 7.5%	0.075 X 10.6 = 0.8 Units	\$61,984 (1)
Lifestyle Commercaill (CL-1)	8.4	8.4 / 82.6 = 10.2%	0.102 X 10.6 = 1.1	\$85,228
Manufacturing/Industrial (M1) Northern Side of Soaring	6.4	6.4 / 82.6 = 7.7%	0.077 X 10.6 = 0.8	\$61,984 (2)
Manufacturing/Industrial (M1) Southern Side of Soaring	6.9	6.9 / 82.6 = 8.4%	0.084 X 10.6 = 0.9	\$69,732 (2)
Business Innovation Zone (BIZ)	4.0	4.0 / 82.6 = 4.8%	0.048 X 10.6 = 0.5 Units	\$38,740
Multi-family Residential (RMW-20)	0	N/A	0	0
Open Space (OS)	0	N/A	0	0
<b>Total</b>	<b>82.6 Units</b>	<b>100%</b>	<b>10.6 Units</b>	<b>\$821,288</b>

**Notes:**

1. The existing winery is a “legal non-comforming use” which will allow the winery to continue its current operation without being subjected to the in-lieu fee outlined in Table 7.4B. If the winery use is discontinued, expands or substantially changes its operations or characteristics, the new or expanded use would be subject to the in-lieu fee identified in Table 7.4B.
2. Some relocated uses to the M1 zoning district may be considered exempt from workforce housing requirements (see M1 zoning district).
3. Payment of the workforce housing in-lieu fee is required prior to issuance of temporary occupancy for the first building within each zoning district for projects approved prior to adoption of the Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments, unless the required number of deed-restricted workforce housing units are constructed onsite to address the project’s unmet workforce housing demand as identified in Table 7.4B. Future projects shall comply with the requirements of Development Code Chapter 18.216 (Workforce Housing). Construction of the workforce housing units is required on the project site or at an off-site location within the Truckee town limits at a location with comparable access to transit and similar services, as well as comparable pedestrian and bicycle access. Construction of the off-site units must be completed prior to issuance of temporary or final certificate of occupancy for the commercial and/or manufacturing/industrial project. Approval of an alternate equivalent proposal shall not fulfill the workforce housing in-lieu fee requirements.

4. The workforce housing in-lieu fee requirements identified in Table 7.4B apply to projects that were approved prior to adoption of the Specific Plan amendments by the Town Council on June 8, 2021 or for projects with vested rights granted or created pursuant to State law prior to the effective date of the Specific Plan amendments. Future projects approved within the Specific Plan area shall be subject to the requirements of Development Code Chapter 18.216 (Workforce Housing). Construction of workforce housing units is required on the project site or at an off-site location within the Truckee town limits at a location with comparable access to transit and similar services, as well as comparable pedestrian and bicycle access. Construction of the off-site units must be completed prior to issuance of temporary or final certificate of occupancy for the commercial and/or manufacturing/industrial project. Payment of in-lieu fees or approval of an alternate equivalent proposal to address the workforce housing demand of future projects shall not be allowed. If Truckee Tahoe Airport Land Use Compatibility Zone restrictions limit the ability of a developer to construct all of the required workforce housing units on-site, the developer may request approval to construct the units off-site.

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# Chapter 8

## Glossary

*Unless otherwise provided herein, the definitions within the Truckee Development Code (Section 18.220) shall apply.*

**Auto / RV / Boat / Motorcycle Repair & Service (small dealership).**

**BIZ.** See “Business Innovation Zone”.

**BP.** See “Business Park”.

**Building Material Sales.** Selling of materials which are used for the purpose of building and construction.

**Building / Structure Frontage.** The building elevation which fronts on a public street, public parking lot, private parking lot available to the general public, or pedestrian walk where customer access to a structure is available.

**Business Innovation Zone.** Land Use designation (BIZ) intended to attract new manufacturing / industrial businesses and provide opportunities for existing local businesses to expand or relocate; including businesses that provide Research and Development, production of local products (custom wood furniture, specialty food/ spirit products), green construction design and material production and other similar uses.

**Business Park.** Land Use designation (BP) intended to attract a variety of business uses that can service the employment and recreational needs of the area such as vehicle/recreational dealerships, warehousing space, specialty food production, light manufacturing, and indoor fitness centers & training facilities .

**California Environmental Quality Act (CEQA).** State law (California Public Resources Code Sections 21000 et seq.) requiring public agencies to document and consider the environmental effects of a proposed action, prior to allowing the action to occur.

**California Public Utilities Commission (CPUC).** The governmental agency which regulates the terms and conditions of public utilities in the State.

**Cohousing.** A type of intentional community composed of private homes supplemented by shared facilities. The community is planned, owned and managed by the residents who also share activities which may include cooking, dining, child care, gardening, and governance of the community. Common facilities may include a kitchen, dining room, laundry, child care facilities, offices, internet access, guest rooms, and recreational features.

**CR.** See “Regional Commercial”.

**CRS.** See “Regional Support Commercial”.

**Detached Cottage Cluster.** See “Small Lot Cottage Housing”.

**Development.** Any construction activity or alteration of the landscape, its terrain contour or vegetation, including the erection or alteration of structures. New development is any construction, or alteration of an existing structure or land use, or establishment of a land use, after the effective date of this Development Code.

**Development Agreement.** A contract between the Town and an applicant for a development project, in compliance with Chapter 18.150 (Development Agreements) of this Development Code and Government Code Sections 65864 et seq. A development agreement is intended to provide assurance to the applicant that an approved project may proceed subject to the policies, rules, regulations, and conditions of approval applicable to the project at the time of approval, regardless of any changes to Town policies, rules, and regulations after project approval. In return, the Town may be assured that the approved project will contain elements and components that are in the best interests of the Town and will promote the public interest and welfare of the Town.

**Development Code.** The Town of Truckee Development Code, Title 18 of the Truckee Municipal Code, referred to herein as “this Development Code.”

**Development Permit.** A document authorizing the holder to proceed with development of a project. s

**District.** See “Zoning District.”

**Duplex.** A detached residential structure under single ownership containing two dwellings.

**Garden Apartment.** Low-rise apartment buildings (3-stories or less) built with landscaped grounds surrounding them. The apartment buildings are often arranged around courtyards that are open at one end. A garden apartment has some characteristics of a townhouse: each apartment has its own building entrance, or just a few apartments share a small foyer or stairwell at each building entrance. Unlike a townhouse, each apartment occupies only one level.

**General Plan.** The Town of Truckee General Plan, including all elements thereof and all amendments thereto, as adopted by the Town Council under the provisions of Government Code Sections 65300 et seq., and referred to in this Development Code as the “General Plan.”

**Greenhouse Farming.** A greenhouse (also called a glasshouse) is a building in which plants are grown using convection heating. These structures range in size from small sheds to very large buildings. A greenhouse is a structure with different types of covering materials, such as a glass or plastic roof and frequently glass or plastic walls. Greenhouse farming also includes vertical greenhouse farms and new growing methods such as hydroponics and aeroponics.

**Green Technology.** A use that is environmentally friendly, developed and used in such a way so that it doesn't disturb the environment and conserves natural resources. Also includes environmental technology and clean technology.

**Grocery Store.** A retail store that primarily sells food. Grocery stores often offer non-perishable food, with some also having fresh produce, butchers, delis, and bakeries. Large grocery stores that stock significant amounts of non-food products, such as clothing and household items, are called supermarkets.

**Housing unit.** Any structure designed or used for the shelter or housing of one or more persons.

**Land use permit.** Authority granted by the Town to use a specified site for a particular purpose, including Conditional Use Permits and Minor Conditional Use Permits, Development Plans and Minor Development Plans, Planned Development Permits, Temporary Use Permits, Variances and Minor Variances, and Zoning Clearances, as established by Article IV (Land Use and Development Permit Procedures) of this Development Code.

**Large Floor Plate.** Indoor floor layout consisting of 5,000 sf or larger in area.

**Lot area.** Gross lot area is the total area included within the lot lines of a lot, exclusive of adjacent dedicated street rights of way. Net lot area is exclusive of vehicular access easements which limit the use of the lot.

**Lot frontage.** The boundary of a lot adjacent to a public street right-of-way.

**Lot line or property line.** Any recorded boundary of a lot. Types of lot lines are as follows:

1. Front lot line. On an interior lot, the property line separating the parcel from the street. The front lot line on a corner lot is the line with the shortest frontage. (If the lot lines of a corner lot are equal in length, the front lot line shall be determined by the Director.) On a through lot, both lot lines are front lot lines and the lot is considered to have no rear lot line.
2. Interior lot line. Any lot line not abutting a street.
3. Rear lot line. A property line that does not intersect the front lot line, which is most distant from and most closely parallel to the front lot line.
4. Side lot line. Any lot line that is not a front or rear lot line.

**Media Production.** Website design and maintenance, graphic design and printing, point of sale systems, point of sale support, and video services.

**Pharmaceuticals.** Of or relating to drugs used in medical treatment.

**Public Improvements and Engineering Standards.** The Town of Truckee Public Improvements and Engineering Standards, as adopted by the Council.

**Regional Commercial.** Land Use designation appropriate for large floorplate commercial and office uses that serve both local and regional markets, including limited restaurant and specialty retail uses.

**Regional Support Commercial.** Land Use designation appropriate for local- and regional-serving retail and service uses that benefit from a highway/Downtown gateway location including recreational vehicle sales, casual dining restaurants, and other retail and service businesses that cannot be easily accommodated downtown. This district supports the CR district by creating additional opportunity for regional service and retail uses.

**Restaurant, Fast Casual.** A type of restaurant that does not offer full table service, but promises a higher quality of food with fewer frozen or processed ingredients than a fast food restaurant (between fast food and casual dining). Counter service accompanied by handmade food is typical. The menu is usually limited to an extended over-counter display. While table service is not offered, conveniences such as non-plastic utensils and plates are common.

**Small Lot Cottage Housing.** Small, modest, single- or double-storied single-family homes with narrow setbacks and small yards, clustered near each other in a group.

**Targeted Land Use.** Targeted Land Uses are those that are considered “preferred” or “highly desirable”. Land uses within this category are identified as those that cannot be easily accommodated in other established or planned commercial areas within Town or significantly contribute to the character of the district. A Development Permit for Targeted Land Uses is **only** required for projects greater than 15,000 s.f. of gross floor area.

**Transportation Center.** A sheltered waiting area located where several bus routes converge that provides a wider selection of destinations through greater transfer opportunities and a permanent presence of transit service.

**Wood Design & Fabrication.** Manufacturing of softwood and hardwood veneer and plywood; structural wood members, and reconstituted wood panel products.

**Zoning Clearance.** Approval from the Planning Division certifying that a proposed structure or use of land meets all applicable requirements of the Town’s Development Code for a specific zoning district.

**Zoning district.** Any of the residential, commercial, industrial, special-purpose, or combining districts established by Article II of this Development Code (Zoning Districts and Allowable Land Uses), within which certain land uses are allowed or prohibited, and certain site planning and development standards are established (e.g., setbacks, height limits, site coverage requirements, etc.).

# Appendix A – 2025 General Plan Consistency Matrix

The following Consistency Matrix is a comprehensive analysis of this Specific Plan’s consistency with the Guiding Principles, Land Use Designations, Goals & Policies of the Town of Truckee 2025 General Plan. It is comprehensive in that it references each applicable component of the 2025 General Plan to PC-3.

General Plan Chapter (Guiding Principles, Goals & Policies)	Joerger Ranch Specific Plan Consistency Summary
Chapter 2 – Land Use Element (Guiding Principles)	
Manage projected growth within the planning period (2005 to 2025). Provide sufficient land identified for development to account for unbuildable residential lots and to insure competition and flexibility in Commercial and Industrial land uses.	Commercial and Industrial land uses are proposed.
Preserve the important qualities of Truckee’s community character through appropriate land use patterns and locations.	Land use patterns and locations are consistent with 2025 General Plan.
Locate significant new development around existing developed areas.	Located adjacent to existing development.
Locate the highest density and intensity of development on infill sites within existing developed areas.	Site is infill site within existing developed area.
Designate an adequate amount of land for commercial and industrial uses to accommodate projected demand and fulfill economic diversification goals. Continue to locate freeway-oriented commercial development only at the existing developed interchanges with Interstate 80 at Donner Pass Road/Cold Stream Road and at State Route 89 South. Ensure high quality design for freeway-oriented commercial development in those areas.	Site designated for commercial and industrial development by 2025 General Plan.  Freeway reference is not applicable.
Prevent “commercial sprawl” in Truckee.	Infill site designated for commercial, industrial, and high density residential development in the 2025 General Plan.
Improve the character and visual quality of development in the Gateway area and along the Brockway Road corridor, and along the Truckee River in the Downtown Study Area.	Character and visual quality will be regulated by Design Guidelines and Specific Plan within Chapter 4 and future Development Permit processes.
Prevent the negative visual impact of mass parking lots and large single building forms.	Design Standards & Guidelines have been provided in Chapter 4.
Promote mixed-use development in Commercial areas. The mix can include office, residential, service and or/retail uses.	Site plan layout provides parking in multiple areas screened by building and landscaping. Larger areas of parking are located towards interior of site, farther from off-site view and screened by buildings.
Ensure adequate provision of public services and infrastructure to support planned residential and non-residential development in suitable locations.	Adequate public services and infrastructure will be constructed in accordance with Chapters 5 and 7.

Chapter 2 – Land Use Element (Land Use Designation)	
<p>Planned Community 3 [PC-3]</p> <p>Planned Community 3 consists of an area identified for future commercial and industrial land uses. Viability of this area for development has been substantially increased by the completion of the Highway 267 bypass in 2002.</p> <p>Land Uses Allowed: Land uses allowed in this designation are commercial, industrial, and high density residential uses.</p> <p>Density and Intensity: An average FAR of 0.20 shall apply to commercial and industrial development. Residential uses are allowed at a maximum density of twelve housing units per acre.</p> <p>Policies: The following policies will guide development of the Specific Plan for PC-3.</p>	<p>Commercial and industrial uses are proposed, Highway 267 Bypass is complete.</p> <p>Proposed land uses are consistent with these allowed uses.</p> <p>Density and intensity of the proposed uses are consistent with these limits within the PC-3 area.</p>
<p>PC-3-P1 Development allowed on the site will be a range of commercial, industrial and residential uses. Services for employees, such as day care facilities and food sales, shall be encouraged.</p>	<p>Specific use charts are provided in Chapter 4 for each land use area.</p>
<p>PC-3-P2 The Specific Plan shall include design standards to provide for architectural consistency of development on the site, in accordance with the Town of Truckee design guidelines.</p>	<p>Design Standards are provided in Chapter 4.</p>
<p>PC-3-P3 Site design shall consider appropriate access to Highway 267, via Brockway Road and Soaring Way, and shall minimize visual impacts from the Highway 267 corridor.</p>	<p>Access to Highway 267 and improvements to Soaring Way and Brockway Road are provided in Chapters 5 &amp; 7. Visual impacts from the Highway 267 corridor are minimized through the dedication of permanent open space, setbacks, and tree retention.</p>
<p>PC-3-P4 The Specific Plan shall include standards for the design of retail shopping areas that avoid “strip commercial” site layout, and that are oriented and scaled to the pedestrian realm.</p>	<p>Design Standards are provided in Chapter 4 to avoid “strip commercial”.</p>
<p>PC3-P5 Specific Plan design standards shall include requirements for parking areas which promote attractive streetscapes, recognize the need for snow storage and removal, and reduce the visual impacts of large, unscreened parking lots through distributed landscaping, landscaped berms and other measures. Parking shall be provided in accordance with the Town of Truckee Design Guidelines.</p>	<p>Design Standards are provided in Chapter 4 that include requirements for parking, snow storage, and landscaping.</p>
<p>PC3-P6 The Specific Plan shall include provisions for supplying, on-site, the required housing for 50 percent of the very-low, low- and moderate-income workforce associated with development of the site. If land use or noise compatibility requirement of the Airport Land Use Compatibility Plan preclude or reduce the total amount of housing that can be developed on PC-3, required workforce housing may be permitted to be located off-site.</p>	<p>The RMW-20 zone includes provisions to meet the workforce housing for the entire Joerger Ranch Specific Plan area. Inclusionary housing requirements have been met through the high density zoning (18 to 20 units per acre) within the RMW-20 zone.</p>
<p>PC3-P7 All development on PC-3 shall support community character goals and policies for the Brockway Road Corridor</p>	<p>Street sections are identified for all roadways within the Specific Plan and are consistent with Town goals &amp; policies.</p>
<p>PC3-P8 Ensure that the mix of land uses in the PC-3 Specific Plan will generate an amount of traffic that, in addition to buildout of the General Plan (considering all planned circulation improvement), would not result in the need for four lanes on Highway 267 between Interstate 80 and the Brockway Road/Soaring Way intersection.</p>	<p>Consistency statement deferred to EIR analysis.</p>

Chapter 2 – Land Use Element (Overlay Designations)	
<p>Policy BR-P1 Preserve and improve the character of the Brockway Road Corridor, including the following aspects:</p> <ul style="list-style-type: none"> <li>• Preservation of the corridor’s open qualities, which provide an important transition from the developed areas of the Town to the open space of the Martis Valley to the south. This could be accomplished through screening with vegetation, use of landscaped buffers or interspersing development nodes with green space and landscaped areas.</li> <li>• Recognition of the corridor’s status as a gateway to Truckee from the south, ensuring that new development and land uses, particularly those proposed for PC-3, support and enhance this role.</li> <li>• Supporting the corridor’s use as a bicycle and pedestrian route through the provision of appropriate facilities for these users, including a recreational trail and Class I bikeway.</li> </ul> <p>Action A6.7 under Goal LU-6 in the Land Use Element and Community Character Element Goal CC-14 and its related policies and actions apply to the Brockway Road Neighborhood Area.</p>	Development proposed by this Specific Plan, as indicated by the Conceptual Master Plan and guided by the Design Guidelines, is consistent with this policy.
Chapter 2 – Land Use Element (Goals & Policies)	
Goal LU-1 Manage growth so as to maintain the unique qualities and character of the Town as a small mountain community.	
P1.1 All new development shall meet important community goals for design quality, open space preservation, and promotion of a livable, sustainable community. Development that does not fulfill these goals shall not be allowed.	Chapter 4 includes design standards that meet community goals for design, open space preservation and sustainable development.
P1.2 Projects that exceed minimum requirements and mandated levels for provision of affordable and workforce housing shall be given a higher priority for development approval. Such projects may be considered for application of less stringent development standards in order to facilitate their development.	The RMW-20 zone allows for high density workforce housing.
Goal LU-2 Provide an adequate amount of land designated for residential, commercial, and industrial uses to meet demand within the life of the Plan.	
P2.1 Ensure adequate supplies of residential, commercial and industrial land, located appropriately, to manage projected growth.	In addition to the studied placement and allotment of commercial and industrial uses within the Plan Area, the Plan has been crafted to allow use and density / intensity flexibility to allow the accurate response to future community demand.
P2.2 Ensure that the primary use of Industrial designated land is for industrial, rather than general commercial uses.	Chapter 4 provides land use standards and design guidelines to meet this policy.
P2.3 Ensure that new residential development meets minimum density standards, based on those described in Section C of the Land Use Element.	Chapter 4 provides land use standards and design guidelines to meet this policy.
Goal LU-3 Create efficient land use patterns which reduce environmental impacts and minimize the potential for residential and commercial sprawl.	
P3.3 To provide for projected population growth in an efficient manner, accommodate development at the highest densities in infill areas, consistent with goals for environmental protection and land use compatibility.	Chapter 4 provides land use standards and design guidelines to meet this policy.

Goal LU-4 Coordinate land development with provision of services and infrastructure.	
P4.2 Cooperate with special districts to plan for and identify suitable future site for needed facilities, including schools, fire stations, solid and liquid waste disposal sites, and utilities infrastructure, so that the local population can be safely and efficiently served, while minimizing potential environmental impacts.	Chapter 5, Transportation, and Chapter 7, Implementation & Phasing, provides consistency with this policy.
Goal LU-5 Encourage a mix of land uses in the Town to promote a vibrant community and to reduce traffic, while addressing the need to minimize land use conflicts.	
P5.4 Discourage new “strip” commercial development and encourage site design for new commercial projects that provides for pedestrian/ bicycle access and proper building scale and proportion relative to the pedestrian realm.	Design Standards are provided in Chapter 4 to avoid “strip commercial”. Chapters 5 and 7 outline pedestrian/bicycle routes consistent with the Towns Trails & Bikeway Master Plan.
P5.7 Require buffering, screening, setbacks, and other measures for new and expanded industrial uses adjacent to residential neighborhoods to minimize impacts and compatibility conflicts.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal LU-6 Preserve and enhance the distinctive community character of Truckee and each of its neighborhoods.	
P6.1 The maximum size limit for a single retail commercial use building shall be 40,000 square feet.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
<p>P6.2 Maintain and enhance Downtown as the heart of Truckee and as the Town’s premier tourist destination through the following methods, and through Action 6.2:</p> <ul style="list-style-type: none"> <li>• Aggressively facilitate pedestrian- oriented development in the Downtown through implementation of the Downtown Specific Plan.</li> <li>• Give some priority in the expenditure of capital improvement funds to projects that will enhance appropriate uses Downtown and facilitate new development, thereby implementing the Downtown Specific Plan.</li> <li>• Allocate staff resources to implement the Downtown Specific Plan.</li> <li>• Actively encourage the relocation of industrial uses from the Downtown area to other more appropriate locations in Town, such as the Pioneer Trail industrial area, or the Airport industrial zone.</li> </ul>	Joerger Ranch Specific Plan provides a receiver site and incentives to relocate industrial uses from the Downtown area.
P6.4 Require buildings to be located closer to the street, where appropriate, and for off-street parking areas to be located to the rear of commercial buildings, where feasible. Ultimate building locations must accommodate snow removal and snow storage, and should maximize solar orientation.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal LU-7 Encourage clustered residential development to create efficient development patterns, and to minimize environmental impacts and threats to public safety.	
<p>P7.1 For all residential developments, require clustering where appropriate. Clustered development as defined in this General Plan includes the following considerations:</p> <ul style="list-style-type: none"> <li>• Clustering of residential development will allow flexibility of site design in responding to the natural features and resources of an individual site.</li> <li>• Clustering means that structures will be located on a site so that larger areas are left as undeveloped open space.</li> <li>• Undeveloped areas may either be preserved in private or public open space, or may be a portion of an individual lot, with deed restrictions prohibiting construction in that portion.</li> </ul>	The RMW-20 zone clusters the residential development portion of project within a 4 acre parcel.

P7.2 Residential development shall be clustered to avoid areas of significant natural resources, including wildlife habitat and migration corridors and visual resources.	The RMW-20 zone clusters the residential development portion of project within a 4 acre parcel.
P7.3 Clustered development types shall be applied within the Town according to the location and character of the development site. Clustered development types and their corresponding recommended locations are summarized in Table LU-7.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P7.4 Clustered development shall incorporate preservation of open space areas as an integral and primary consideration in the overall development plan for a site. Considerations in preserving open space through clustering shall include the following: <ul style="list-style-type: none"> <li>• Maximizing preservation of open space types that reflect the Town's priorities as stated in the Conservation and Open Space Element.</li> <li>• Maintaining an appropriate relationship of the site to the character and context of adjacent neighborhood areas and nearby and adjoining open space areas.</li> <li>• Respecting individual site features and characteristics, including topography, natural features, natural hazards and constraints, and the presence of sensitive biological resources.</li> </ul>	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P7.5 Preserve the portions of parcels not developed with clustered residential used as undeveloped open space. Preservation and management options for open space include: <ul style="list-style-type: none"> <li>• Dedication to a homeowners association.</li> <li>• Dedication to a public agency such as the Parks District, or to a land trust or other non-profit agency.</li> <li>• Use of building envelopes in conjunction with conservation easements or deed restrictions.</li> </ul>	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.  More than 20 acres of permanent open space are provided within the plan area.
Goal LU-9 Support development patterns in the Planning Area that do not negatively impact the Town of Truckee, and that enhance the quality of life for residents of Truckee and the wider region.	
P9.1 Support clustered development within the Planning Area.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P9.3 Oppose any development in the Planning Area that would necessitate widening of Highway 267 north of Brockway Road to four lanes	Joerger Ranch Specific Plan does not necessitate the widening of Highway 267 north of Brockway Road.
P9.5 Support provision of housing for all income segments dispersed evenly throughout all developed portions of the Planning Area.	The RMW-20 zone provides workforce housing for the entire Joerger Ranch Specific Plan area.
Chapter 3 – Community Character Element (Guiding Principles)	
Maintain Truckee's unique qualities and sense of place to preserve the town's established historic and scenic mountain town character.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Conserve and protect the natural beauty, scenic landscapes and open space resources of the Town, including the Truckee River, Donner Lake, ridges and hillsides, scenic corridors and vistas, and views of the night sky.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Emphasize and enhance the visual and physical connection between the town's natural environment and the community's quality of life.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Promote the highest possible standards of town design, planning and architecture in Truckee.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Create a diverse, complementary hierarchy of mixed-use and neighborhood centers to strengthen Truckee's economic base and provide focal points for the town and its neighborhoods.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.

<p>Improve and strengthen connections between the town's neighborhoods and centers.</p>	<p>Chapter 5, Transportation, and Chapter 7, Implementation &amp; Phasing, provides consistency with this policy.</p>
<p>Reinforce the unique qualities of Truckee's neighborhoods.</p>	<p>Chapter 4, Land Use Standards &amp; Design Guidelines provides consistency with this policy.</p>
<p>Enhance Truckee's important corridors and community gateways.</p>	<p>Chapter 5, Transportation, and Chapter 7, Implementation &amp; Phasing, provides consistency with this policy.</p>
<p>Protect Truckee's historic and cultural resources.</p>	<p>Chapter 4, Land Use Standards &amp; Design Guidelines provides consistency with this policy.</p>
<p>Chapter 3 – Community Character Element (Overlay Designations)</p>	
<p><b>B. Town Design - Other Mixed Use Centers</b></p> <p>Mixed use centers are defined as areas that include a mixture of residential, retail and office development. In terms of town design, mixed use centers have come to be recognized as one of the key components of traditional development that makes it vibrant and successful. They not only provide a human presence and support for commercial areas and businesses, but also provide opportunities for people to live in proximity to where they work and shop, reducing dependence on the automobile.</p> <p>At present, the Downtown and Gateway Area are the only two mixed-use centers in Truckee, containing a broad range of commercial and residential uses within them. However, this General Plan recognizes the importance and value of developing new mixed-use centers in Truckee. As described in the Land Use Element, three new mixed use centers will be developed in Truckee, in Planned Community 1 (PC-1), Planned Community 3 (PC-3), and as part of the Gray's Crossing Planned Development. PC-1 will likely include a mixture of residential, commercial (retail and office) and lodging uses, and PC-3 will provide both a major new grocery store and new light industrial space for Truckee, as well as clustered apartments and employee housing. Gray's Crossing is to include a mixed-use "Village Center" that will provide attached dwelling units as well as retail and office space.</p>	<p>Joerger Ranch Specific Plan provides consistency with this policy.</p>
<p><b>B. Town Design – Neighborhood Centers</b></p> <p>Neighborhood centers are small local-serving centers, typically providing between 4,000 to 6,000 square feet of commercial space, although are sometimes larger when they also serve a broader customer base from drive-by trade. Uses primarily consist of a mix of convenience-oriented retail, small-business offices and residential uses. Typically located at a key intersection or along a major roadway within a residential area, neighborhood centers provide close and convenient access to the goods and services needed on a daily basis by local residents.</p> <p>While densities in some neighborhoods may not provide a sufficient customer base to support a commercial center, all neighborhoods would benefit from the addition or strengthening of a center or focal point. Such a focal point could encompass a variety of amenities, from a small neighborhood park, to elements as simple as a newspaper stand or community bulletin board, combined with some strategically located benches and a water fountain. A small node like this would be appropriate for many neighborhoods in Truckee, providing a place to stop and rest on a neighborhood stroll or dog walk, and an opportunity for social interaction among neighbors.</p>	<p>Joerger Ranch Specific Plan provides consistency with this policy.</p>

<p>B. Town Design - Corridors and Gateways</p> <p>Corridors and linear features, located along major thoroughfares, which connect various parts of the community. Although Truckee has many corridors associated with its roadway network, this Element focuses on a more limited number of corridors where its is felt to be most critical to define visual character and quality, particularly in light of changes that may occur in conjunction with development allowed under this General Plan. The corridors and gateways discussed in this section are mapped in Figure CC-3.</p> <p>Gateways are a particular type of corridor feature, with special status due to their important role in conveying first impressions to visitors and a sense of arrival or transition from one place to another. Gateways can also be a point of orientation for the town, providing information to motorists by directing them to areas of interest and by providing insights into the historical, cultural and economic foundations of the area. It should be noted that some of Truckee's gateways are located along the key corridors discussed in the following section, and in those cases, are addressed under the discussion of the corridor in question.</p>	<p>Joerger Ranch Specific Plan provides consistency with this policy.</p>
<p>Corridors</p> <p>This section describes a number of key corridors in Truckee, including the Brockway Road Corridor, West River Street Corridor, and Donner Pass Road Corridor in the Donner Lake area. Other important corridors in Truckee, including the Gateway corridor and designated scenic corridors along Interstate 80 and Highway 89 North, are addressed elsewhere this Element. Natural and linear open space corridors such as that associated with the Truckee River and other waterways , are also discussed briefly in this Element, and in greater detail in the Conservation and Open Space Element.</p> <ul style="list-style-type: none"> <li>• Brockway Road Corridor This corridor extends from the Truckee River to the new intersection with Highway 267 at the PC-3 development site. A variety of open space areas interspersed with residential and commercial development can be found along the length of the corridor, contributing to a varied, mixed-use character. Open space areas along Brockway Road include the Ponderosa Golf Course, Truckee River Regional Park, and some meadowland areas. Existing residential and commercial development varies in the degree to which it is visible, as well as in the quality of its architecture.</li> <li>• There are presently no sidewalks along Brockway Road, but a multi-use trail is planned along the northeast side of the corridor to connect the development along the road with the Truckee River Regional Park, surrounding neighborhoods and the Downtown, as well as PC-3 and the Airport area. Overhead power lines are visible along most of the corridor's length.</li> </ul> <p>This General Plan allows for a significant amount of development along Brockway Road, in recognition of its status as a key town arterial, and its proximity to the Downtown and Highway 267. However, the most significant change along this corridor is likely to occur in conjunction with the development of PC-3, as described in the Land Use Element. The development site is situated at the key intersection of Highway 267 with Brockway Road, and will provide a significant presence at the intersection and the south end of Brockway Road, which serves as a key gateway to Truckee from the south. The identification of an appropriate visual character for development at PC3, in keeping with the desired character of the corridor as a whole, is thus a critical concern of the goals, policies and actions within this Element.</p>	<p>Joerger Ranch Specific Plan provides consistency with this policy.</p>

<p>Gateways</p> <p>While a gateway can be defined as any major entry or access point to the community, this Community Character Element deals with a more limited number of gateways that are of strategic importance to the town. Other more informally-defined gateways include the points of entry to Truckee along the Interstate and by means of its access ramps as well as the State highway corridors that provide entries to Truckee from the region. In many cases, these gateways are signified by the transition from the rural mountain surrounds of the previously traversed landscape to the more developed environs within the Town limits. On a more localized scale, the north and south entrances to the historic Downtown are defined by an abrupt shift in the topography, and character and scale of buildings, creating a strong sense of arrival into Truckee’s historic center.</p> <p>Important gateways in Truckee are:  Highway 267 South Gateway. As described above, the Brockway Road/ Highway 267 intersection is the most significant gateway to Truckee from the south. The development of PC-3 will help to define the quality and character of this gateway to create a distinct and positive sense of arrival in Truckee along this important route.</p>	<p>Joerger Ranch Specific Plan provides consistency with this policy.</p>
<p>Chapter 3 – Community Character Element (Goals &amp; Policies)</p>	
<p>Goal CC-1 Preserve open space in Truckee that contributes to the town’s scenic mountain community character.</p>	
<p>P1.2 Ensure that all new development, occurring at all scales and densities, maximizes the provision of all types of open space, including scenic open space that contributes to and enhances the town’s community character.</p>	<p>More than 20 acres of permanent open space are provided within the Joerger Ranch Specific Plan.</p>
<p>P1.3 Cluster new development so as to preserve the maximum amount of desired types of open space, as identified in the Conservation and Open Space Element.</p>	<p>The Land Use Zoning Exhibit Map for Joerger Ranch provides consistency with this policy. New development is clustered within specific zones to allow for large areas of permanent open space.</p>
<p>P1.4 Create a connected network of open spaces in Truckee that is accessible to the community for outdoor recreation and other use and enjoyment, as a key aspect of local community character.</p>	<p>The Land Use Zoning Exhibit Map for Joerger Ranch provides consistency with this policy. New development is clustered within specific zones to allow for large areas of permanent open space.</p>
<p>Goal CC-2 Preserve the natural beauty of Truckee, including the Town’s scenic resources, views and vistas, and the visual quality of the town’s steep slopes, ridge and bluff lines, and hillsides.</p>	
<p>P2.4 Ensure that new development in Truckee’s lowland areas, including its forested areas and meadowlands, and the Truckee River Valley, contributes to and enhances the scenic quality and visual harmony of the built environment that comprises the Truckee townscape.</p>	<p>Chapter 4, Land Use Standards &amp; Design Guidelines provides consistency with this policy.</p>
<p>P2.7 Require electric, telecommunications and cable television facilities serving new development to be installed underground wherever possible. Where undergrounding is impractical, above ground antennae and telephone and high voltage transmission lines shall be located out of significant scenic vistas.</p>	<p>All new electric, telecommunications and cable television within Joerger Ranch shall be installed underground.</p>
<p>P2.9 Encourage the planting and maintenance of roadside landscaping and the use of landscaping elements where appropriate along major public thoroughfares.</p>	<p>Chapter 4, Land Use Standards &amp; Design Guidelines provides consistency with this policy.</p>
<p>P2.10 Encourage the preservation of trees and native vegetation, including specimen trees, in development projects.</p>	<p>Chapter 4, Land Use Standards &amp; Design Guidelines provides consistency with this policy.</p>

Goal CC-4 Protect views of the night sky and minimize the effects of light pollution.	
P4.1 Recognize and preserve views of the night sky as an important natural and scenic resource in Truckee.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P4.2 Require light fixtures to be designed and sited so as to minimize light pollution, glare, and light trespass into adjoining properties.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal CC-5 Maintain the town's unique community character, including a high standard of town design in all development in Truckee.	
P5.1 Ensure that planning and development decisions are oriented towards the maintenance of Truckee's unique character, reflecting the following considerations. <ul style="list-style-type: none"> <li>• Identification of specific types of centers, residential neighborhoods, employment districts, corridors and gateways.</li> <li>• Respect for the quality, character and context of existing development within these different areas of the town.</li> <li>• Ensuring that new development enhances the desired character of each of these areas.</li> <li>• Discouraging new architecture that directly mimics or is derivative of the buildings of the historic downtown.</li> <li>• Encouraging the retrofit or rehabilitation of existing buildings to more closely comply with Town policies, standards and guidelines for high quality architecture and design.</li> <li>• Consideration of the relationship of the built environment to the qualities and context of the landscape and natural environment in which it is situated.</li> </ul>	Joerger Ranch Specific Plan provides consistency with this policy.
P5.2 Require all new development to incorporate high quality site design, architecture, and planning so as to enhance the overall quality of the built environment in Truckee and create a visually interesting and aesthetically pleasing town environment.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P5.3 Prohibit franchise and corporate architecture in commercial buildings, except in special situations.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P5.5 Enhance physical connections between adjacent uses and between different parts of Truckee.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P5.6 Regulate the size, quantity, location and design of signs to maintain and enhance the visual appearance of the town.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P5.9 For all new development in Truckee, consider how the integration of trees and native landscaping can contribute to the overall quality of development-specific design and the town's unique character.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal CC-9 Create mixed-use centers that can provide local shopping, services and employment in proximity to housing.	
P9.2 Encourage all existing and new mixed-use centers to create a "park once environment" that provides a variety of uses within walking distance.	Joerger Ranch Specific Plan provides consistency with this policy.
P9.3 Require new mixed-use centers to provide housing of a variety of types and affordability, including employee housing, unless determined to be incompatible.	The RMW-20 zone provides workforce housing at a density which will allow affordability for employees.
P9.4 Enhance pedestrian connections from nearby residential areas to local shopping centers so as to enhance the mixed-use quality of Truckee's commercial centers and their surrounding residential areas; reduce the need to drive to access daily needed goods and services; and provide safe and convenient pedestrian connections.	Chapter 5, Transportation, and Chapter 7, Implementation & Phasing, provides consistency with this policy.

P9.5 Require new mixed use centers to incorporate site planning and design that reflects walkability and opportunities for indoor and outdoor social interaction, including clustered buildings, parking dispersed in smaller lots, as well as pedestrian-scale design features.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal CC-11 Strengthen and improve the character of Truckee's town residential neighborhoods.	
P11.1 Encourage new residential developments adjacent in appropriate locations (e.g. planned community areas) and with appropriate site conditions to include the use of neo-traditional site design and architectural elements. These encompass elements such as a grid, or modified grid street layout, narrower streets, street trees, detached sidewalks, and neo-traditional house designs in keeping with Truckee architecture and design.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P11.2 Discourage architectural monotony between individual units within a suburban subdivision or residential development project.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P11.6 Encourage preservation and creation of open space areas within town residential neighborhoods, particularly in the form of small neighborhood parks, pocket parks, trails, and greenways.	Chapter 4, Land Use Standards & Design Guidelines, creation of permanent open space, and construction of trails provides consistency with this policy.
Goal CC-13 Ensure that Truckee's commercial and industrial districts are safe, well-designed and accessible areas that are positively integrated with other parts of the town.	
P13.1 Sustain high design standards for new commercial development in employment districts, particularly where development is visible from along major arterial and roadways.	Chapter 4, Land Use Standards & Design Guidelines and creation of permanent open space along Highway 267 provides consistency with this policy.
P13.2 Ensure that new development within Truckee's light industrial areas and employment districts is designed in a way that is sensitive to adjacent residential development.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P13.3 Create live-work opportunities, as appropriate, within employment districts.	Although the Plan Area is not located within a designated employment district, live-work opportunities are allowed in all zones.
P13.4 Coordinate with the Truckee-Tahoe Airport District and Placer County to ensure visually compatible development of light industrial and office uses located within the town around the airport.	Joerger Ranch Specific Plan provides consistency with this policy.
Goal CC-14 Enhance the character and qualities of the Brockway Road Corridor as a gateway to Truckee.	
P14.1 Preserve and enhance the open character and qualities of the Brockway Road Corridor by including appropriate buffering and screening from the road corridor, and interspersing development areas or nodes with green space.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P14.3 Ensure that the design quality and character of the PC-3 development is compatible with the gateway qualities of the south end of Brockway.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal CC-17 Preserve and enhance all entryways and gateways to the Town to create a distinct sense of arrival and positive image of Truckee.	
P17.1 Design gateway design treatments for key entryways into the Town that incorporate landscaping, signage, public art, and/or structural elements that communicate a sense of arrival in Truckee. Gateways for which such treatments should be applied include: <ul style="list-style-type: none"> <li>• Donner Pass Road where it enters Downtown Truckee, near the Highway 89 North intersection.</li> <li>• At the Brockway Road/Highway 267, intersection, as described in Action A14.3, above.</li> <li>• Along West River Street.</li> <li>• Along Highway 267 South, near the Mousehole.</li> <li>• At the western Donner Pass Road freeway interchange.</li> </ul>	Chapter 5, Transportation, and Chapter 7, Implementation & Phasing, provides consistency with this policy.

Goal CC-19 Identify and protect archaeological and paleontological resources that enrich our understanding of Truckee's early history and the early cultures and environment of the region.	
P19.1 As part of the development review process, require proper archaeological or paleontological surveying, testing, research, documentation, monitoring and safe retrieval of archaeological and cultural resources.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P19.2 Require an archaeological survey by a qualified professional whenever there is evidence of an archaeological or paleontological site within a proposed project area, is determined to be a high likelihood for occurrence of such sites, or where a project involves substantial site disturbance.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P19.3 Consult with representatives of the Native American community whenever necessary to ensure the respectful treatment of Native American sacred places.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal CC-20 Strengthen the social fabric of the Truckee community.	
P20.2 Encourage opportunities for social interaction and community activity through the creation of well-designed, accessible public spaces located throughout the town, and by encouraging commercial uses that support community interaction, such as cafes and restaurants, performance and arts spaces, and child activity centers.	Joerger Ranch Specific Plan provides consistency with this policy.
P20.3 Support the work of community institutions, government agencies and non-profits that provide support services to Truckee residents.	Joerger Ranch Specific Plan provides an opportunity for a non-profit center at Hope Court/Brockway Road.
Chapter 4 – Circulation Element (Guiding Principles)	
Coordinate land use and transportation planning in Truckee to provide for the long-range development of the Town's roadway system consistent with the existing and future land use patterns described in the Land Use Element.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Maintain acceptable traffic operations on the Town's roads through application of Level of Service thresholds, and by conditioning new development on the ability of local roads and intersections to accommodate projected traffic impacts.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Eliminate, to the extent feasible, all traffic signals in Truckee.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Minimize the negative impacts of transportation infrastructure upon Truckee's community character, local neighborhoods, and the environment.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Ensure that new development minimizes impacts on the roadway network, is integrated into the existing transportation system and provides opportunities for use of alternate modes.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Work cooperatively with adjacent jurisdictions to address regional traffic issues.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Reduce automobile travel demand to reduce impacts on the Town's roadway system, lessen the need for new or expanded road facilities to accommodate increased demand, and decrease pollutants emissions from automobiles.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy..

Provide a safe, comprehensive, and integrated system of trails and bikeways as a key component of the circulation system.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Promote a safe and efficient transit system, including both bus and rail, to reduce congestion, improve the environment, and provide viable alternatives to the automobile.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Chapter 4 – Circulation Element (Goals & Policies)	
Goal CIR-1 Develop a roadway system that provides access and mobility for Truckee residents and businesses and adequately serves existing and planned land uses while maintaining local community character.	
P1.1 Maintain a hierarchy of arterial, collector and local roadways in Truckee by planning, designing, and regulating roadways in accordance with the functional classification system described in this Element.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
P1.2 Undertake improvements to the Town's roadway network, as identified in the Circulation Plan in Figure CIR-3 and in Section D, to ensure that the access and mobility needs of existing and future land uses can be adequately served.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
P1.3 Ensure that right-of-way for cumulatively needed future roadway improvements is reserved or acquired as part of relevant project approvals.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
P1.4 Improve existing roadways in Truckee to meet current Town engineering standards.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
P1.5 Ensure that existing and future roadway, sidewalk, and bikeway standards, and the implementation of such standards, take Truckee's climatic conditions into account.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
P1.6 Ensure that the design and functionality of the Town roadway network in keeping with the goals and policies of this General Plan concerning community character. Examples of such policies include those listed under Goal CC-3, as well as policies for the Gateway Area, Brockway Road, and West River Street in the Community Character Element.	Chapter 5, Transportation, and Chapter 6, Public Services & Infrastructure, provides consistency with this policy.
Goal CIR-2 Maintain adequate Level of Service on Truckee's roadways and intersections to ensure the safe and efficient movement of people and goods throughout the Town.	
P2.1 Establish and maintain a Level of Service D or better on road segments and for total intersection movements in portions of the Town outside of the Downtown Specific Plan Area.  Establish and maintain a Level of Service E or better on arterial and collector road segments and for total intersection movements within the Downtown Specific Plan Area.  Throughout the Town, individual turning movements at unsignalized intersections shall not be allowed to reach LOS F and to exceed a cumulative vehicle delay of four vehicle hours. Both of these conditions shall be met for traffic operations to be considered unacceptable.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P2.2 In addition to the standards described in Policy 2.1, the criteria and thresholds shown in Table CIR-6 shall be applied to future development projects to determine the need for a traffic impact analysis to be conducted and to determine if a project's traffic impact is found to be significant.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.

<p>P2.4 Improve connectivity throughout the Town's roadway network, through roadway improvements, while minimizing environmental, circulation, and residential neighborhood impacts. This should include:</p> <ul style="list-style-type: none"> <li>• New and improved links between roadways of the same classification.</li> <li>• New and/or improved links between higher and lower capacity roadways where such connections would not negatively impact the lower capacity roadway's operations or local neighborhood character, would be consistent with community character and environmental goals described elsewhere in the General Plan, and would not result in redesignation of a lower classification roadway to a higher classification, unless shown as such on the Circulation Plan.</li> <li>• Discouraging the use of local and residential neighborhood roadways as through routes, particularly for commercial and industrial traffic.</li> <li>• Requiring that new development maximizes connectivity of local streets within the development itself, and makes connections to the adjacent street network and neighborhood areas.</li> </ul>	<p>Chapter 5, Transportation, Chapter 6, Public Services &amp; Infrastructure, and Chapter 7 Implementation &amp; Phasing provides consistency with this policy.</p>
<p>Goal CIR-3 Minimize the impacts of new development on the existing roadway network.</p>	
<p>P3.1 Require the preparation of traffic impact analyses to identify impacts and mitigation measures for projects that may result in significant traffic impact, as specified in Table CIR-6. In these analyses, Level of Service shall be computed according to the planning methodology documented in Special Report 209: Highway Capacity Manual, published by the Transportation Research Board in 2000, or as amended in subsequent updates. Cumulative impacts shall be modeled buildout of the General Plan.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>P3.2 Require the assessment of construction-related project impacts in traffic impact analyses that assess and adequately mitigates the effect of construction traffic on the roadway network, as well as any potential disruption to or re-routing of traffic that might be needed during project construction.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>P3.3 Require all new development projects to adequately mitigate identified impacts through construction of improvements and/or payment of traffic impact mitigation fees. Mitigation of significant project-related impacts may require improvements beyond those addressed by the current Capital Improvement Program and traffic impact mitigation fee program.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>P3.4 Ensure that new streets and roads are dedicated and constructed according to roadway design and access standards adopted by the Town.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>Goal CIR-4 Create new developments that are integrated into the circulation network and promote connectivity within and between community areas.</p>	
<p>P4.1 Require transportation systems planned and constructed in conjunction with significant development projects, including roads, trails, bikeways, and other improvements, to provide links to the existing transportation network</p>	<p>Chapter 5, Transportation, Chapter 6, Public Services &amp; Infrastructure, and Chapter 7 Implementation &amp; Phasing provides consistency with this policy.</p>
<p>P4.2 Require planning for land use and transportation systems in new growth areas that provides opportunities for residents and employees to accomplish many of their trips by walking, bicycling or using transit.</p>	<p>Chapter 5, Transportation, Chapter 6, Public Services &amp; Infrastructure, and Chapter 7 Implementation &amp; Phasing provides consistency with this policy.</p>

Goal CIR-5 Provide adequate funding for construction, improvement and maintenance of existing and new roadways	
<p>P5.1 Charge a traffic impact fee that is set in direct proportion to the level of impact identified in a traffic impact analysis, so as to ensure that the actual cost of improvements demanded individually or cumulatively by a project can be met. Such fees shall be applied to improvements needed to increase the capacity of the roadway network including:</p> <ul style="list-style-type: none"> <li>• Improvements listed in Table CIR-5</li> <li>• State Transportation Improvements Projects (STIP) projects identified in Caltrans' Regional Transportation Plan.</li> <li>• Improvements such as widening of travel lanes to meet Town engineering standards, and to add Class II and II bike facilities.</li> <li>• Other improvements identified by the Town needed to increase the capacity of the roadway system, including those determined to be needed through development of the Downtown Traffic Study, and to reduce delays and improve safety at railroad crossings</li> </ul>	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P5.3 Require projects to undertake needed roadway improvements on public streets fronting or adjoining the development site as part of the project, and to make available right-of-way needed for future public improvements.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
Goal CIR-6 Minimize potentially adverse impacts of transportation infrastructure and parking facilities on Truckee's community character and important environmental and cultural resources.	
P6.1 Locate, construct, and maintain new roads and roadways improvements so as to prevent adverse impacts to water quality and significant biological, scenic, and historic resources.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P6.2 Use road and intersection improvement projects as an opportunity to improve the aesthetic quality of the intersection or roadway in question. Such improvements could include sidewalk installation, landscaping, medians, and improved street lighting or pavement treatment.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P6.4 Maintain Highway 267 between Interstate 80 and the Brockway Road /Soaring Way intersection at two lanes	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P6.5 Maintain roadways in Truckee at a maximum of two travel lanes. Exceptions to this policy include Interstate 80, Highway 89 South, Highway 267 from Truckee Airport Road/Shaffer Mill Road to Brockway Road/Soaring Way, and Coldstream Road.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P6.6 Avoid intersection improvements that would include dual left turn lanes.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P6.8 Ensure that adequate parking is provided for commercial, residential and other land uses in Truckee, while, at the same time, limiting excess off-street parking.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal CIR-7 Utilize roundabouts instead of traffic signals throughout Truckee	
P7.1 Strive to replace existing traffic signals with roundabouts as a means of intersection control, including traffic signals on State Highways.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.

P7.2 Install roundabouts instead of new traffic signals or capacity-enhancing improvements to existing signalized intersections, when roundabouts will achieve the same or better Level of Service as a traffic signal, where it is physically feasible to do so, and when installation of the roundabout will not be substantially costlier than a signal.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P7.3 When traffic calming is desirable at unsignalized intersections, encourage roundabouts instead of multi-lane stop controlled intersections, or the addition of extra turn lanes when the roundabout will achieve the same or better level of service, where it is feasible to do so, and when installation of the roundabout will not be substantially costlier.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
Goal CIR-9 Reduce vehicle trips as a means to minimize demands on the existing roadway system, reduce the future need for new or expanded road facilities, and reduce energy consumption and air pollution.	
P9.2 Promote land use and transportation strategies that will reduce automobile trips, particularly implementation of compact, pedestrian-oriented development, mixed uses, live-work projects, neighborhood-serving commercial and mixed use centers, and clustered and infill development.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Goal CIR-10 Provide a safe, comprehensive, and integrated system of facilities for pedestrians and cyclists.	
P10.1 In planning the Town's transportation system, strive for a more balanced system that provides alternatives to the automobile while still meeting the Level of Service standards expressed in this Element.	Figure 5-11, Trails Exhibit provides consistency with this policy.
P10.3 Identify and implement new pedestrian facilities beyond those identified in the Trails and Bikeways Master Plan and Downtown Streetscape Plan. These facilities may include, but not be limited to, pedestrian facilities along Donner Pass Road between Cold Stream Road and South Shore Drive, along Highway 89 South, and along West River Street.	Figure 5-11, Trails Exhibit provides consistency with this policy.
P10.4 Ensure that streetscape and urban design plans for the Gateway corridor and Brockway Road include pedestrian connections to the Downtown's pedestrian network.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.
P10.5 Link new trails and bikeways with other bikeways, parks and open space areas to provide safe and continuous routes.	Figure 5-11, Trails Exhibit provides consistency with this policy.
P10.6 Use road and intersection improvements as an opportunity to improve bicycle and pedestrian facilities.	Chapter 5, Transportation, , and Chapter 7 Implementation & Phasing provides consistency with this policy.
P10.10 Require major development projects to include pedestrian facilities and bikeways.	Figure 5-11, Trails Exhibit provides consistency with this policy.
P10.11 Enforce pedestrian and bicycle access standards for all new development and require developers to finance and install pedestrian walkways, equestrian trails and multi-use trails in new development, as appropriate and necessary to address circulation needs.	Chapter 5, Transportation, , and Chapter 7 Implementation & Phasing provides consistency with this policy.
P10.12 Provide facilities that separate bicycle traffic from vehicular traffic whenever it is feasible to do so.	Chapter 5, Transportation, , and Chapter 7 Implementation & Phasing provides consistency with this policy.
Goal CIR-11 Enhance the existing bus and rail transit system in Truckee.	
P11.1 Require new development to incorporate features that encourage transit use, including shelters and safe routes to transit stops, and ensure that right-of-way for future transit access is reserved in plans for new growth areas.	Chapter 5, Transportation, Chapter 6, Public Services & Infrastructure, and Chapter 7 Implementation & Phasing provides consistency with this policy.

Goal CIR-13 Allow for safe and efficient aviation operations at the Truckee-Tahoe Airport that are compatible with surrounding land uses.	
P13.3 Ensure consistency of the General Plan with the Airport Land Use Compatibility Plan, as adopted by the Foothill Airport Land Use Commission in December 2004, and implement the Land Use Compatibility Plan to ensure protection of airport operations from incompatible land uses.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P13.4 Support mitigation efforts to reduce interior noise levels of residential dwellings significantly impacted by aircraft operations.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Chapter 5 – Affordable Housing (Guiding Principles)	
Provide an adequate number of sites for the development of housing to meet Truckee’s fair share of the regional housing need.	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre.
Provide housing to meet the needs of very-low, low- and moderate income residents, particularly those who work in Truckee, while maintaining the Town’s character.	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre. This density and a restriction on the maximum unit size (750 sf ) will ensure affordability to lower income residents.
Enhance and promote quality housing design in both new and existing Development.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Chapter 5 – Affordable Housing (Goals & Policies)	
Goal H-1 Ensure an adequate supply of housing to meet the housing needs of all segments of the community.	
Policy H-1.1 Provide adequate sites for the production of new residential units to meet the needs of existing and future residents.	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre.
Policy H-1.2 Provide for a sufficient amount of higher density residential land, distributed throughout the Town, to accommodate the Town’s share of the regional housing need for affordable housing.	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre.
Policy H-1.3 Encourage the private sector to build affordable housing	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre.
Goal H-4 Balance the need and provision of housing in the community with its impacts on the environment and needed public facilities and services.	
Policy H-4.1 Encourage residential design that promotes energy efficiency and sustainable building practices.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Policy H-4.2 Encourage clustered residential development that reduces infrastructure and other development costs, preserves and enhance important environmental resources, and maintains important areas as open space.	The RMW-20 zone provides 4 acres for construction of workforce housing at a density of 18 to 20 units per acre.
Chapter 6 – Economic Development Element (Guiding Principles)	
Build upon the Town’s existing assets to diversify and strengthen the local economy in ways that are appropriate and responsive to Truckee’s context and natural environment.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Develop high-wage jobs in Truckee that enable the local workforce to both live and work in the Town.	The Regional Commercial (CR), a Lifestyle Commercial (CL) and a Business Innovation Zone (BIZ) zoning districts provide consistency with this policy.

Focus business attraction, retention, and expansion efforts on key economic sectors that have the greatest likelihood of success in Truckee.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Maintain and enhance community quality of life as a key competitive advantage.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Promote and enhance the Town's role as a year-round tourist destination.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Chapter 6 – Economic Development Element (Goals & Policies)	
Goal ED-1 Foster high-quality economic development.	
P1.3 In reviewing development projects, consider a project's ability to fulfill economic development guiding principles and goals for the Town, including fostering desired industries, providing living wage jobs, and upholding Truckee's small-town, mountain character as a key competitive advantage.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Goal ED-2 Encourage high wage light industrial development in Truckee.	
P2.1 Provide sufficient business park and light industrial space to allow for the attraction and expansion of quality employers within Truckee.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
P2.2 Encourage uses consistent with high wage and high density employment in business parks and light industrial areas, as opposed to services or low employment density uses such as warehouse space.	The BIZ zoning district provides consistency with this policy.
P2.3 Ensure that land designated Industrial is used primarily for industrial, rather than general commercial/retail uses.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
Goal ED-3 Encourage the development of knowledge based "new economy" businesses.	
P3.1 Support the creation of new office space that is well-designed and offers amenities and telecommunications infrastructure attractive to new economy business enterprises.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Goal ED-5 Promote and enhance Truckee's role as a sustainable year-round tourist destination.	
P5.3 Support new visitor-oriented restaurants, lodging, and services to meet tourist needs and capture expenditures locally.	The CR zoning and Chapter 4, Land Use Standards & Design Guidelines provide consistency with this policy.
Goal ED-6 Foster a local environment that attracts and develops a skilled workforce through educational opportunities, cultural facilities and availability of affordable housing.	
P6.2 Support the inclusion of cultural facilities as a component of new mixed-use development proposed within the Town as an amenity that makes Truckee attractive to a vibrant, innovative business community.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Goal ED-7 Leverage Truckee's "natural capital" in implementing economic development strategies.	
P7.1 Land use, urban design, open space and community facilities in Truckee shall reinforce the preservation and enhancement of the Town's natural capital.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
P7.3 Support the establishment of businesses and industries that draw upon Truckee's natural assets and environment.	Joerger Ranch Specific Plan provisions provides consistency with this policy.
Goal ED-8 Maintain the vitality of Downtown Truckee.	
P8.4 Provide opportunities for industrial uses currently located along the Truckee River in the Downtown to relocate to other, more suitable locations.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.

Chapter 7 – Conservation and Open Space Element, Guiding Principles	
Increase the amount of permanently protected, connected, and publicly accessible open space in and around Truckee for the use and enjoyment of all Truckee residents.	The OS zoning, Chapter 4, Land Use Standards & Design Guidelines, and the proposed Trail network (Figure 5-11) provides consistency with this policy.
Protect sensitive biological resources, specifically special status plants and wildlife, streams and wetlands, and significant wildlife movement corridors.	Joerger Ranch Specific Plan has been designed to avoid identified biological resources such as wetlands.
Maintain and improve air quality in the Truckee region to ensure a healthful environment for all.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal COS-9 Link open space areas in Truckee through a well-connected network of open space corridors and trails.	
P9.2 Support the development and construction of a town-wide system of trails and bikeways, including, as priorities, the development of the Donner Lake/Truckee River parkway (see Goal COS-10), and the establishment of trails linking the Downtown with the Gateway Area and surrounding developed areas.	The Trails Exhibit (Figure 5-11) identifies bike paths to be constructed as part of Joerger Ranch. Construction of these trails provide consistency with this policy.
P9.3 Require new development to incorporate trail corridors identified in the Trails and Bikeways Master Plan into the overall project site plan.	The Trails Exhibit (Figure 5-11) identifies bike paths to be constructed as part of Joerger Ranch. Construction of these trails provide consistency with this policy.
P9.4 Preserve existing open space corridors, and connections to adjacent open space areas, and integrate publicly accessible trails and open space corridors into new development to the extent feasible.	More than 20 acres of permanent Open Space is provided within the Joerger Ranch Specific Plan area. Construction of the trails shown in Figure 5-11 will provide public access into new development.
Goal COS-11 Protect water quality and quantity in creeks, lakes, natural drainages and groundwater basins.	
P11.1 Minimize excessive paving that negatively impacts surface water runoff and groundwater recharge rates.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P11.2 Protect surface and groundwater resources from contamination from runoff containing pollutants and sediment, through implementation of the Regional Water Quality Control Board's (RWQCB) Lahontan Region's, Best Management Practices.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P11.5 Require new development projects that have the potential to impact local water quality through increased storm water runoff or erosion to include analysis of water quality impacts as a component of project review, and to integrate mitigation measures that would reduce identified impacts to an acceptable level.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P11.6 Utilize Low Impact Development and Best Management Practices established in the Regional Water Quality Control Board's Truckee River Hydrologic Unit Project Guidelines for Erosion Control, and the State of California Stormwater Best Management Practices Handbooks, and other resources such as the Practice of Low Impact Development (US Department of Housing and Urban Development) and Water Quality Model Code and Guidebook (State of Oregon, Department of Land Conservation and Development) as guidelines for water quality and erosion control measures required by the Town.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P11.7 Ensure that all proposed developments can be adequately served by available water supplies.	TDPUD has indicated the proposed development can be adequately served with water. Chapter 6, Public Services & Infrastructure provides consistency with this policy.

P11.9 Recognize the importance of stormwater management in protecting all water resources in Truckee, for example, flood control, surface and ground water quality, and river, stream and lake health.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal COS-12 Protect the Town's soil resources from erosion.	
P12.2 Require projects that require earthwork and grading, including cuts and fills for roads, to incorporate measures to minimize erosion and sedimentation. Typical measures include project design that conforms with natural contours and site topography, maximizing retention of natural vegetation, and implementing erosion control Best Management Practices.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal COS-13 Reduce particulate matter pollution in Truckee to meet State and federal ambient air quality standards.	
P13.1 Require multi-family residential, commercial, industrial, subdivisions and other discretionary development to maintain consistency with the goals, policies and control strategies of the Town's Particulate Matter Air Quality Management Plan.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P13.3 Require all construction projects to implement dust control measures to reduce particulate matter emissions due to disturbance of exposed top-soils. Such measures would include watering of active areas where disturbance occurs, covering haul loads, maintaining clean access roads, and cleaning the wheels of construction vehicles accessing disturbed areas of the site.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal COS-14 Reduce emissions of air contaminants and minimize public exposure to toxic, hazardous and odoriferous air pollutants.	
P14.1 Minimize potential impacts created by unpleasant odors, as well as other airborne pollutants from industrial and commercial developments.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P14.2 Prohibit sensitive receptors such as residential uses, schools and hospitals, from locating in the vicinity of industrial and commercial uses known to emit toxic, hazardous or odoriferous air pollutants, and prohibit the establishment of such uses in the vicinity of sensitive receptors.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P14.3 Reduce automobile dependence, thereby reducing greenhouse gas emissions, by encouraging mixed land use patterns that locate services such as banks, child care facilities, schools, neighborhood shopping centers and restaurants in close proximity to employment centers and residential neighborhoods.	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.
P14.4 Review all discretionary development applications to determine the need for pedestrian/bike paths connecting to adjacent development and services, in order to provide alternatives to automobile transportation.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P14.9 Require new development with the potential to generate significant quantities of ozone precursor air pollutants to be analyzed in accordance with guidelines provided by the NSAQMD and appropriate mitigation be applied to the project to minimize these emissions.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal COS-15 Encourage conservation of energy and fuel resources, strive to reduce generation of solid waste, and promote environmental sustainability.	
P15.5 Encourage new private and public development to maximize opportunities for use of passive or natural heating and cooling and encourage sites with solar opportunities to be designed with natural heating and cooling principles	Chapter 4, Land Use Standards & Design Guidelines provides consistency with this policy.

Chapter 8 – Noise Element (Guiding Principles)	
Preserve Truckee’s peaceful mountain environment by minimizing community exposure to noise.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Maintain consistency with the noise criteria and policies of the Truckee Tahoe Airport Land Use Compatibility Plan as they apply in the airport environs.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Chapter 8 – Noise Element (Goals & Policies)	
Goal N-1 Minimize community exposure to excessive noise by ensuring compatible land uses relative to noise sources.	
P1.1 Allow new development only if consistent with the ground transportation noise compatibility guidelines illustrated in Figure N-3 and the policies of this Element. Noise measurements used in establishing compatibility shall be measured in dBA CNEL and based on worst-case noise levels, either existing or future, with future noise levels to be predicted based on projected 2025 levels.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P1.2 Require new development to mitigate exterior noise to “normally acceptable” levels in outdoor areas where quiet is a benefit, such as in the backyards of single-family homes.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P1.3 Enforce the California Noise Insulation Standards for interior noise levels attributable to exterior sources for all proposed new single- and multi-family residences.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P1.5 Allow land uses within Normally Unacceptable categories only where the allowed use can be shown to serve the greater public interests of the citizens of Truckee.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P1.6 When considering development proposals in the environs of the Truckee Tahoe Airport, enforce the noise compatibility criteria and policies set for the in the adopted Truckee Tahoe Airport Land Use Compatibility Plan.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal N-2 Address noise issues through the planning and permitting process.	
P2.1 Require mitigation of all significant noise impacts as a condition of project approval.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P2.2 Require preparation of a noise analysis/acoustical study, which is to include recommendations for mitigation, for all proposed projects which may result in potentially significant noise impacts to nearby noise sensitive land uses such as residences.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P2.3 Require preparation of a noise analysis/acoustical study, which is to include recommendations for mitigation, for all proposed development within noise-impacted areas that may be exposed to levels greater than “normally acceptable.”	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
P2.4 Discourage the construction of sound walls and require development projects to evaluate site design techniques, building setbacks, earthen berms, alternative architectural layouts and other means to meet noise reduction requirements.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.
Goal N-3 Reduce noise levels from sources such as domestic uses, construction and car stereos, and from mobile sources, including motor vehicle traffic and aircraft operations.	
P3.1 Enforce provisions of the Municipal Noise Ordinance, which limits maximum permitted noise levels that cross property lines and impact adjacent land uses.	Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.

<p>P3.13 Require the following standard construction noise control measures to be included as requirements at construction sites in order to minimize construction noise impacts.</p> <ul style="list-style-type: none"> <li>• Equip all internal combustion engine driven equipment with intake and exhaust mufflers that are in good condition and appropriate for the equipment.</li> <li>• Locate stationary noise generating equipment as far as possible from sensitive receptors when sensitive receptors adjoin or are near a construction project area.</li> <li>• Utilize “quiet” air compressors and other stationary noise-generating where appropriate technology exists.</li> <li>• The project sponsor shall designate a “disturbance coordinator” who would be responsible for responding to any local complaints about construction noise. The disturbance coordinator will determine the cause of the noise complaint (e.g., starting too early, bad muffler, etc.) and will require that reasonable measures warranted to correct the problem be implemented. The project sponsor shall also post a telephone number for excessive noise complaints in conspicuous locations in the vicinity of the project site. Additionally, the project sponsor shall send a notice to neighbors in the project vicinity with information on the construction schedule and the telephone number for noise complaints.</li> </ul>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>Chapter 9 – Safety Element (Guiding Principles)</p>	
<p>Minimize the potential risk to life and property from natural and induced hazards in the Town of Truckee.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>Chapter 9 – Safety Element (Goals &amp; Policies)</p>	
<p>Goal SAF-2 Reduce hazards associated with flooding</p>	
<p>P2.3 Incorporate stormwater drainage systems in development projects to effectively control the rate and amount of runoff, so as to prevent increases in downstream flooding potential.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>Goal SAF-4 Protect lives and property from risks associated with wildland and urban fire.</p>	
<p>P4.4 Require new development to incorporate adequate emergency water flow, emergency vehicle access and evacuation routes.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>
<p>P4.7 Ensure that the development review process addresses wildland fire risk, including assessment of both construction- and project-related fire risks particularly in areas of the Town most susceptible to fire hazards. Cooperate with the TFPD in reviewing fire safety plans and provisions in new development, including aspects such as emergency access, site design for maintenance of defensible space, and use of non-combustible materials.</p>	<p>Joerger Ranch EIR and Mitigation Measures provide consistency with this policy.</p>

## PROPOSED ZONING STATISTICS

JOERGER RANCH - PC-3 ZONING DESIGNATION	ACREAGE	MAX. DENSITY
 REGIONAL COMMERCIAL (CR)	16.3 AC.	0.25 FAR
 LIFESTYLE COMMERCIAL (CL)	4.7 AC.	0.25 FAR
 MANUFACTURING / INDUSTRIAL (M1)	9.5 AC	0.25 FAR
 BUSINESS INNOVATION ZONE (BIZ)	11.2 AC	0.25 FAR
 MULTI-FAMILY RESIDENTIAL (RMW-20)	4.0 AC	20 UNITS / ACRE
 OPEN SPACE (OS-P)	20.9 AC	N/A
TOTAL:	66.6 AC.	

## Exhibit A Proposed Zoning Exhibit

January, 2015

Note: \*Martis Drive is conceptually located and the area within proposed Martis Drive right-of-way is excluded from the total acreages.

